

RIVER ARTS DISTRICT TRANSPORTATION PROJECT

A section of the Wilma Dykeman RiverWay
Buncombe County, North Carolina

TIP NO: U-5019

WBS No. 41503.1.1

FEDERAL AID PROJECT NO: STP-1302(37)

Administrative Action

CATEGORICAL EXCLUSION

January 24, 2013

United States Department of Transportation

Federal Highway Administration

And

The City of Asheville & North Carolina Department of Transportation



Submitted Pursuant to the National Environmental Policy Act 42 USC 4332(2)(C)

And

49 U.S.C. 303

Approved

1.24.13

Date

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Manager
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1-24-13

Date

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PROJECT COMMITMENTS

RIVER ARTS DISTRICT TRANSPORTATION PROJECT
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Commitments Developed Through Project Development and Design:

Floodplains, NCDOT Hydraulics Unit

- The City of Asheville, in coordination with the NCDOT Hydraulics Unit, will coordinate with the NC Floodplain Mapping Program (FMP), to determine the status of the project with regard to applicability of NCDOT Memorandum of Agreement, or approval of a Conditional Letter of Map Revision (CLOMR) and subsequent final Letter of Map Revision (LOMR).
- This project involves construction activities on or adjacent to FEMA-regulated stream(s). Therefore, the City shall submit sealed as-built construction plans to the Hydraulics Unit upon completion of the project construction, certifying that the drainage structures and roadway embankment that are located within the 100-year floodplain were built as shown on the construction plans, both horizontally and vertically.

Work Zone Traffic Control

- The City of Asheville, in coordination with the NCDOT as appropriate, will coordinate with local media during the construction of the project to alert the public of traffic restrictions and construction related activities.

Archaeology

- In locations where ground disturbance is proposed to expand to a depth greater than one meter, such as culverts, a professional archaeologist will either monitor construction or, prior to construction, will conduct archaeological testing with assistance of a backhoe at those locations.

Norfolk Southern Railroad Coordination

- The City of Asheville will contact Norfolk Southern during final design to verify and negotiate at-grade crossings and right-of-way needs for the project. Roadway profiles will allow Norfolk Southern to retain their spur track along Riverside Drive near Hill Street and existing at-grade crossings at Lyman and Roberts Streets. All information related to the railroad (such as track alignment, horizontal and vertical clearances at existing tracks, traffic control plan, and flagging protection requirements) will be submitted to Norfolk Southern for verification prior to construction.

Landscaping & Art

- The landscape and amenity plans will follow the River District Design Guidelines that are part of the City of Asheville's adopted Unified Development Ordinance Sec. 7-8-19.



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CATEGORICAL EXCLUSION FOR THE RIVER ARTS DISTRICT TRANSPORTATION IMPROVEMENT PROJECT

A section of the Wilma Dykeman RiverWay in Asheville, Buncombe County, North Carolina

TIP U-5019

I. DESCRIPTION OF PROPOSED ACTION

A. General Description: Wilma Dykeman RiverWay

The Wilma Dykeman RiverWay is a planned 17-mile corridor along the French Broad River and Swannanoa River in Asheville, Buncombe County. The RiverWay concept will link areas along the two rivers into a “continuous multi-access parkway.” RiverLink’s 2004 *Wilma Dykeman RiverWay Master Plan* identified the vision for the overall 17-mile corridor. Four primary goals for the RiverWay were defined:

- To provide frontage for economic development along its length, whether it is adaptive reuse of existing historic structures or the development of new recreational structures for civic and residential uses;
- To facilitate the continual expansion of Asheville’s greenway system by extending the pedestrian network linking neighborhoods to the rivers as well as to civic, recreational, and cultural destinations;
- To provide a new transportation spine that will enhance the ability to interconnect local streets and regional transportation arteries; and
- To improve and enhance the river corridors by implementing ecologically sustainable technologies and practices.

The Plan divided the corridor into seven development nodes. It is within one of these sections that the reference ‘River Arts District’ was born to describe the area and the intended character of the district that has become the focus of this transportation improvement project.

B. General Description: River Arts District Transportation Improvement Project

This multimodal transportation improvement project, led by the City of Asheville, will focus on a 2.2-mile section of the larger Wilma Dykeman RiverWay, including the corridor of Lyman Street (city street) and a portion of Riverside Drive (city street) between Amboy Road (SR 3556) and Hill Street (SR 1231). The streets run east of and parallel to the French Broad River through the River Arts District, historically an industrial area that has been redeveloped with cafes, artist studios, and other attractions in recent years. The study area is shown in **Figure 1** in **Appendix A**. This project is included in the NCDOT 2012-2020 *State Transportation Improvement Program* (STIP) as Project U-5019. The STIP indicates the project is programmed for planning and environmental studies only and the studies will be guided by the City of Asheville. Final design for the project is partly funded by an FTA TIGER grant and is



anticipated to begin in early 2013. Right-of-way acquisition and construction phases are currently unfunded.

This planning/environmental study is funded by a USDOT grant administered through the NCDOT; therefore the study will comply with the National Environmental Policy Act (NEPA) of 1969, as amended. NEPA requires that potential adverse and beneficial social, cultural, economic, and natural environmental impacts of the project be evaluated as part of the project development process. This Categorical Exclusion (CE) has been developed in accordance with NEPA, and in accordance with the North Carolina State Environmental Policy Act of 1971 (SEPA), as amended.

The northern terminus of the 2.2-mile transportation improvement project is at the Hill Street/Riverside Drive intersection. The southern terminus is the intersection of Lyman Street with Amboy Road. The western boundary of the Study Area is essentially coterminous with the eastern bank of the French Broad River. The eastern boundary of the Study Area abuts commercial and residential development, industrial land use, and the Norfolk Southern railroad. These limits represent logical termini, as illustrated in **Table 1**.

Table 1: Logical Termini Requirements

1. Be of sufficient length to address environmental matters on a broad scope
- The 2.2-mile project length encompasses sufficient distance to contain a wide variety of environmental considerations present in the River Arts District.
2. Have independent utility or significance
- The 2.2-mile project length represents local streets that connect to state-maintained highways at both termini.
- The Amboy Road intersection (south) provides a natural decision-point for vehicular traffic, connecting to a major east-west corridor in southern Asheville and providing a river crossing.
- The Hill Street intersection (north) provides a natural decision-point for vehicular traffic, connecting to I-26 and local streets north of the interstate.
- Both Amboy Road and Hill Street provide planned connections for cyclists and pedestrians, connecting to other local bicycle and pedestrian networks.
3. Do not restrict consideration of alternatives for other reasonably foreseeable transportation improvements.
- The 2.2-mile project length provides adequate area to consider a range of alternatives.
- The project alternatives are being developed to be consistent with corridor-level improvements identified in the <i>Wilma Dykeman RiverWay Master Plan</i> .

This is the first planning/environmental study undertaken along the larger Wilma Dykeman RiverWay corridor. The project examined transportation needs along Lyman Street and Riverside Drive to identify an improvement strategy that is consistent with the vision for the River Arts District in the *Master Plan*.

C. Historical Resume: Context of Recent Planning Efforts

The proposed transportation improvement project is occurring within a rich history of planning efforts that are intended to preserve and enhance the French Broad River in this location. Documented planning efforts start with the 1920s *Asheville Plan* by John Nolan, followed by the 1950s environmental



advocacy book *The French Broad* by Wilma Dykeman. In more recent decades, the 1980s *Riverfront Plan* and the *Asheville Riverfront Open Space Design Guidelines* illustrated the future vision for the area.

To respond to the revitalizing River Arts District, a number of more current transportation planning studies completed near the study area have expressed the multimodal needs within the River Arts District. The project was developed to be consistent with many of these local plans set by area entities. The City of Asheville, the French Broad Metropolitan Planning Organization, Buncombe County, and other local agencies have established planning documents that incorporate future transportation needs for the area. The multimodal aspects for this particular project incorporate concepts that are set forth in many of the plans. For the region, the following plans help define the multimodal vision for the riverfront corridor: the 2005 *City of Asheville Pedestrian Plan*, the FBRMPO *Long Range Transportation Plan 2030*, the *Asheville City Development Plan 2025*, the city's *Comprehensive Bicycle Plan*, the *Asheville Downtown Master Plan*, the *Asheville River Development Plan*, the 2012 *Buncombe County Greenway Master Plan*, the city's 2009 *Transit Master Plan*, and the 2009 *City of Asheville Parks, Recreation, Cultural Arts and Greenways Master Plan*.

While the proposed River Arts District Transportation Improvement Project is not intended to implement each of these concepts in their entirety, consideration was given to these previously defined community visions so that the resulting project could integrate compatible elements from these plans.

II. EXISTING CONDITIONS, PURPOSE, AND NEED FOR PROJECT

A. Purpose of the Project

The primary purpose of the proposed transportation improvement project is:

- To improve the existing roadway geometric deficiencies along Riverside Drive and Lyman Street in the River Arts District; and
- To enhance the multimodal mobility and system linkages (vehicular, pedestrian, bicycle, and transit) along Lyman Street and Riverside Drive by providing efficient and convenient access from Amboy Road to Hill Street.

The improvements would enhance safety and provide additional modal options for the traveling public and visitors to the River Arts District. These modal and service connections would improve regional mobility, particularly for pedestrians and bicyclists.

B. Need for the Project

The need for the project is built upon its existing roadway geometric deficiencies and multimodal mobility and system linkages. These needs are described in the following subsections, followed by a broader description of existing transportation conditions within the study area.

Roadway Geometric Deficiencies

Riverside Drive and Lyman Street are two lane local roadways running north-south through the project area. Speed limits are posted at 30 mph. Numerous driveways and stop-controlled intersections are



located along the corridor; signalized intersections are located at the southern terminus (Lyman Street at Amboy Road) and at Riverside Drive at Haywood Street/Craven Street. With relatively low traffic volumes, improving traffic flow is not a major need in the project area; however, there are substandard geometric elements (such as the curve immediately west of the Lyman Street/Riverside Drive intersection and the railroad overpass with low vertical clearance near the northern project terminus) which limit sight distance and truck movements.

Additionally, drainage is an issue along the corridor, which partially lies within the French Broad floodway. The entirety of the project falls within the 100-year floodplain. The existing Riverside Drive and Lyman Street roadways do not include stormwater infrastructure or ditches. Low-lying portions of Riverside Drive often experience standing water in the roadway following rainfall events.

Mobility and Linkages

The River Arts District Transportation Improvement Project responds to a range of multimodal transportation-related needs along Riverside Drive and Lyman Street within the River Arts District. A variety of facilities are provided in the study area for motorists, transit riders, cyclists, and pedestrians as discussed below.

Bicycle Network: Currently, 4-foot bike lanes are marked to run along the shoulders of Lyman Street from Amboy Road to the sharp curve approaching the intersection with Riverside Drive. There are no other bicycle facilities within the project area but there is a newly constructed bicycle climbing lane along Clingman Avenue nearby.

Portions of two NCDOT-designated bike routes run through the study area: Riverview (Route 10) crosses the river at Amboy Road and travels north along Lyman Street to Clingman Avenue before crossing the river again along Haywood Road. Emerald Necklace (Route 1) follows Riverside Drive from Clingman Avenue through the project area, connecting it to areas to the north and east. Proposed improvements have been identified as separate projects along Roberts Street, across the Haywood Street/Craven Street bridge, and across the Amboy Road bridge to provide improved bicyclist connections throughout the area.

Sidewalk Network: Limited pedestrian facilities exist within the project area; a few connections are scattered throughout the River Arts District, with sidewalks and crosswalks along portions of Depot Street, Clingman Avenue, and Roberts Street. A short section of sidewalk is provided along the east-west segment of Lyman Street but nowhere else along either route within the project area. Three bridges across the French Broad River provide pedestrian facilities: I-240, Craven Street, and the RiverLink Bridge (Haywood Road). A fourth structure provides a pedestrian link over I-26, from Roberts Street to residential areas north of the interstate.

Proposed pedestrian improvements have been identified as separate projects along Roberts Street, Hill Street, across the Craven Street bridge, and across the Amboy Road bridge to provide improved pedestrian connections throughout the area.



Greenway Network: Multi-use trails as part of the greenway network are proposed along both sides of the French Broad River as identified in the 2004 *Master Plan* and the 2009 *Greenways Master Plan* stretch. Currently, portions of these greenways/multi-use trails have been constructed only within the French Broad River Park on the west side of the river, beyond the project area. No multi-use trails exist on the east side of the river within the project area.

In addition to these modes, private railroad lines and a “blueway” run the length of the project area as well. The French Broad River is a popular blueway route for canoes and kayakers. Currently within the River Arts District section of the Wilma Dykeman RiverWay, there are only two river access locations: one on the western bank located at the northeast corner of the French Broad River Park and one on the eastern bank located underneath the Haywood Road Bridge at Jean Webb Park. There is an opportunity within the project area to build upon the efforts from RiverLink to provide additional and meaningful river access as part of the evolving French Broad River (FBR) Paddle Trail. The overall intent of the FBR Paddle Trail is to provide additional river access and recreational opportunities, including riverside camping, along its entire 116 mile stretch throughout Western North Carolina. Aside from the camping opportunities, additional and appropriately located river access within the River Arts District can provide a unique experience through one of Asheville’s sought after and thriving areas, increase environmental awareness and stewardship, and have a positive economic impact with access to the arts as well as existing and future redevelopment including mixed-use residential, retail, commercial, office and restaurants.

Asheville Redefines Transit, the City’s bus service, provides five transit stops within a quarter mile walking distance of the study area. These stops are located along four different routes: W1, W2, N3, and C. Route C travels through the southern portion of the project area, following Meadow Road to Amboy Road. Future plans identified in the City’s *Transit Master Plan* include two routes serving the future Wilma Dykeman RiverWay, a UNC-A-AB Tech intercampus shuttle, and an AB Tech intercampus shuttle. Portions of the future routes are expected to run along Riverside Drive and Lyman Street. Based on transit performance and new priorities, the City expects to update the *Transit Master Plan* in 2015 to address ridership needs and location of bus stops. Today, school buses rarely if ever travel through the project area. However, Mountain Mobility provides demand-response transit service within the area and private tour buses and trolleys offer routes to and through the River Arts District.

Developing an interconnected, multimodal transportation network along the River Arts District has been identified as a need in the study area, as documented in the numerous planning documents described in Section I.C. A public survey was conducted in September 2011; when asked to identify the top five mobility issues in the River Arts District, over 80% of respondents identified pedestrian safety as a top need. **Figure 2** in **Appendix A** summarizes responses received to this question. Written responses indicated that there are existing safety problems along Riverside Drive and Lyman Street that will compound as the area develops. The need for additional signage is cited in multiple responses received.

The transportation improvements within the study area will facilitate connectivity and transfers between modes and services, improve pedestrian and bicycle facilities, and provide the public and



visitors with a variety of modal choices. In conjunction with the planned system improvements, overall mobility will improve for all regional transportation system users.

C. Description of Existing Roadway Conditions

This section provides an overview of the existing geometric, functional, and operational characteristics of the transportation system within the project area.

1. Functional Classification

Riverside Drive and Lyman Street are classified as minor arterial local roadways.

2. Physical Description

Table 2 summarizes key existing physical elements of Riverside Drive and Lyman Street within the project area.

Table 2: Physical Description of Existing Roadways

Feature	Riverside Drive	Lyman Street
Lane width	Two 12-foot lanes	Two 12-foot lanes
Shoulder type	None	None
Medians	None	None
Horizontal Alignment	Minimum Radius 500 ft	Minimum Radius 500 ft
Vertical Alignment	Flat	Flat
Right-of-Way Width	45-60 ft	45-60 ft
Access Control	None	None
Posted Speed Limit	30 mph	30 mph
Intersections	At Hill Street/I-26 ramps Signal at Craven Street At Lyman Street	At 5-Points At Riverside Drive At Old Lyman Street (north) At Old Lyman Street (south) Signal at Amboy Road
Interchanges	None	None
Railroad Crossings	1 (at grade)	1 (at grade)
Structures (along roadway)	None	None
Structures (overpasses)	Southern Railroad Bridge (low 13-foot clearance) I-26/I-240 Parallel Bridges (adequate clearance) Haywood St/Craven St Bridge (access from Riverside Dr) RiverLink Bridge (adequate clearance)	Amboy Road Bridge (access from Lyman St)
Bike paths	None	Striped along roadway from Amboy Rd to Curve Studios
Sidewalks	At southern terminus surrounding 12-Bones	Between 5-Points intersection and Curve Studios



Table 2: Physical Description of Existing Roadways

Feature	Riverside Drive	Lyman Street
Utilities	Overhead power lines Water/Sewer by City Gas by PSNC Phone/Cable by Charter, ATT	Overhead power lines Water/Sewer by City Gas by PSNC Phone/Cable by Charter, ATT

3. School Bus Usage

Currently, school buses do not routinely travel to or through the project area.

4. Traffic Carrying Capacity

Existing Traffic Volumes & Level of Service: Existing traffic counts were conducted in June 2010 at key intersections; peak hour volumes are shown in **Figure 3** in **Appendix A**. Traffic operations at these intersections were analyzed based on methodologies developed in the *Highway Capacity Manual* using Synchro 7.0 software; the full traffic analysis is documented in the *Capacity Analysis Report* prepared for the project, dated March 2012.

Level of service (LOS) is a term used to represent different traffic conditions, assigning a grade A through F based on driver perceptions of delay and traffic flow. LOS A represents free flow movement while LOS F represents extreme congestion; LOS D or better is generally considered acceptable for urban street operations. For signalized intersections, LOS is calculated for the entire intersection, but for unsignalized intersections, LOS is calculated for stop-controlled approaches. **Table 3** presents 2010 LOS information for the five key intersections analyzed in the project area. Generally each intersection operates at LOS C or better for both peak hours based on observed 2010 traffic volumes.

Table 3: 2010 Level of Service and Delay

Intersection	AM Peak	PM Peak
Riverside Drive / Hill Street (westbound approach)	LOS B 14.1 sec	LOS C 23.8 sec
Riverside Drive / US 19/23 On-Ramp (southbound left turn movement)	LOS A 8.3 sec	LOS A 9.6 sec
Riverside Drive / Craven Street (entire intersection)	LOS B 11.6 sec	LOS C 24.5 sec
Riverside Drive / Lyman Street (southbound approach)	LOS B 12.5 sec	LOS B 13.4 sec
Lyman Street / Amboy Road (entire intersection)	LOS B 13.9 sec	LOS A 9.4 sec

Future Traffic Volumes & LOS: Traffic volumes were projected to future year 2035 based on the French Broad River Travel Demand Model, which includes improvements to the regional transportation network shown in the French Broad River 2035 Travel Demand Model but no improvements in the project area. The I-26 Connector Project, which would link the I-26/I-40/I-240 interchange southwest of



the city to the US 19/US 23/US 70 north of the city, was not included in the model since no timeline for implementation has been identified. **Figure 4** shows projected future volumes in the project area for this “Future No Build” scenario. As shown in **Table 4**, LOS degrades substantially at each study area intersection due to the increased traffic volumes traveling along the local roadway connections.

Table 4: 2035 No Build Level of Service and Delay

Intersection	AM Peak	PM Peak
Riverside Drive / Hill Street (westbound approach)	LOS F 81.3 sec	LOS D 29.7 sec
Riverside Drive / US 19/23 On-Ramp (southbound left turn movement)	LOS B 11.0 sec	LOS B 11.6 sec
Riverside Drive / Craven Street (entire intersection)	LOS C 23.8 sec	LOS E 64.0 sec
Riverside Drive / Lyman Street (southbound approach)	LOS F 802.1 sec	LOS F 364.3 sec
Lyman Street / Amboy Road (entire intersection)	LOS D 38.2 sec	LOS B 13.9 sec

5. Accident Data

Because both streets are locally maintained, crash statistics were not available. However, input from stakeholders indicates safety is a concern, particularly related to the lack of pedestrian facilities. Far fewer individuals identified driver safety as a key issue.

6. Airports

The Asheville Regional Airport is the nearest airport to the study area, located approximately 9 miles south of the study area. No special consideration is needed for this transportation mode.

D. Other Highway Projects, Transportation, and Land Use Plans

Currently, there are three multimodal projects and one roadway improvement project listed in the STIP in close vicinity to the project area. These projects include:

- U-4739, Amboy Road from Meadow Road to I-240: widening existing roadway to multi-lanes with a bridge replacement over the French Broad River at the intersection of Amboy Road and Lyman Street.
- E-4585: installing sidewalks and pedestrian features on Amboy Road between Carrier Bridge and Michigan Avenue. [Completed]
- EB-3608: constructing an off-road bicycle path along the French Broad River (one mile) off of Amboy Road to Hominy Creek. [Completed]

In addition, NCDOT proposes improvements to the interstate highway system to connect I-26 from the I-26/I-240/I-40 interchange southwest of Asheville to US 19-23-70 north of Asheville (STIP I-2513). The project improves existing I-240 plus a portion on new alignment. The project is part of the larger system of improvements to connect I-26 with I-81 south of Kingsport, Tennessee. It will reduce delays and



congestion on the current I-240 bridges over the French Broad River, which currently operate at capacity; the project will also help to improve safety and extend the useful service life of the I-240 Bridges. Although no implementation timeline has been identified at this time, the project is currently in the environmental analysis phase of the project development process.

In addition, three private developments are either proposed or under development in the project vicinity.

- New Belgium Brewery purchased an 18-acre riverfront lot on the west side of the French Broad River between the I-26 and RiverLink bridges. Beginning construction in 2013, the brewery plans to construct a new facility that will employ an estimated 150 people by its full build out in 2020.
- Glen Rock Depot is under development by Mountain Housing Opportunities. The site includes retail space and 60 apartments. A future phase of the project calls for the adaptive reuse of the historic Glen Rock Hotel on Depot Street east of the study area, renovating the building to create commercial, office, and retail spaces.
- Private developers submitted plans to the city for a new restaurant, which will be located just south of the Norfolk-Southern railroad bridge. Construction is anticipated to start in early 2013.

E. Other Benefits for Project

Beyond the primary purposes for the project, a number of secondary goals have been identified through coordination with the City, stakeholders, and the public. These goals are listed below with a short description.

- Alternatives should be consistent with previous plans. This includes a variety of documents which have been developed to describe the community's vision for the River Arts District, including the *Wilma Dykeman RiverWay Master Plan*, the *City's 2025 City Development Plan*, and others.
- Alternatives should create a unique gateway. The *City's Wilma Dykeman RiverWay Master Plan* identifies the River Arts District as a signature greenway/gateway to the City.
- Alternatives should connect adjacent neighborhoods. Nearby neighborhoods include Hillcrest, Montford, West Asheville, WECAN, Downtown Asheville, South French Broad, and others.
- Alternatives should promote "smart growth" principles. These principles include measures to balance economic development, a range of residential needs, environmental sustainability, and healthy communities. This might include multimodal transportation choices, livable communities initiatives, mixed use developments, or other elements.
- Alternatives should increase multimodal choices. In addition to traveling by car, opportunities should be available to travel by foot, by bike, by bus, by boat, or other mode. Opportunities to transfer between modes (e.g. park-n-ride lots) should also be considered.
- Alternatives should enhance economic competitiveness and promote sustainable redevelopment. This aligns with the vision described in the *Wilma Dykeman RiverWay Master Plan* and the *City's 2025 City Development Plan*.



- Alternatives should provide recreational opportunities. This could include river-related activities like fishing or kayaking, opportunities to view scenic vistas, or greenways and access to local parks.
- Alternatives should improve environmental protection and water quality. Implementing a riparian buffer, improving stormwater management, or enhancing wildlife habitats are example strategies to fulfill this goal.
- Alternatives should develop opportunities for streetscapes, amenities, and public art. This could include street trees, benches, scenic overlooks, performance space, piazzas, signage, visual art, or any other number of examples. For projects within Asheville, the city's goal is that 1% of the total construction budget goes to incorporating public art into city projects.
- Alternatives should minimize conflict points for cyclists and pedestrians. To improve safety, improved railroad crossings or crosswalks should be considered.
- Alternatives should preserve future mobility through access management. Median breaks, intersection improvements, and the "urban village" street design concept incorporate this goal.

III. PROPOSED IMPROVEMENTS

A. Preliminary Study Alternatives

A number of preliminary alternatives were considered to address the purpose of and needs for the project. These preliminary study alternatives included Travel Demand Management (TDM) strategies (relatively low-cost measures that reduce travel demand to improve traffic flow), mass transit, Transportation Systems Management (TSM) strategies (relatively low-cost measures that improve traffic flow by increasing network efficiency), improving the existing facility, and construction alternatives on new alignment. The TDM, mass transit, and TSM alternatives were dismissed early in the process because they would not fully satisfy the project purpose.

In addition, the No Build Alternative serves as a baseline for comparison between other alternatives. It includes routine maintenance of existing roadways. Although this alternative does not meet the project purpose to improve existing roadway geometric deficiencies and enhance multimodal mobility/system linkages, it was carried forward in the analysis to provide a comparison.

A variety of build alternatives were developed to improve existing geometric deficiencies and enhance multimodal mobility/system linkages along Riverside Drive and Lyman Street in the River Arts District. These alternatives are shown in **Figure 5** in **Appendix A**. Generally, the Yellow Alternative follows the existing Riverside Drive and Lyman Street alignments. The Orange Alternative was developed to most closely describe the corridor vision defined in the *Wilma Dykeman RiverWay Master Plan*.

The typical section for each of these build alternatives includes two 11-foot wide travel lanes, 5-foot wide bike lanes, a median, and 5-foot wide sidewalks offset from the street by a vegetated strip. In some locations, on-street parking is also included. The design speed is 30 mph, with a posted speed limit of 25 mph. Roadways and intersections will be designed to comply with both AASHTO and NCDOT design standards for local roadways. A 10-12 foot wide greenway/multi-use path along the riverfront is



also included for each alternative, consistent with the defined vision for the area. A selection of typical sections along the build alternatives is shown in **Figure 6** in **Appendix A**.

B. Detailed Study Alternatives

The No Build Alternative and six preliminary study Build Alternatives described above were screened against the project purpose, secondary project goals, and Section 4(f) resources in the study area. The intent of this screening was to eliminate any infeasible alternatives from consideration before detailed study was undertaken to define impacts.

The purpose forms the basis for the decision-making process: each alternative must meet the purpose and address each of the identified needs to be considered a viable solution. Of the six preliminary study Build Alternatives, all partially or wholly meet both elements of the project purpose by improving deficiencies (e.g. drainage facilities and the sharp curve near 12 Bones) and enhancing mobility/linkages. Although the No Build Alternative does not meet the purpose, it was carried forward as a baseline for comparison.

Following the purpose screening, alternatives were screened against the other goals for the project, described in **Section II.E**. These factors describe other values, issues, and concerns beyond the primary transportation needs that were considered. All six build alternatives satisfy the secondary goals.

Screening of Alternatives against Section 4(f) Resources

Following these screening efforts, the No Build and six preliminary study Build Alternatives were screened against Section 4(f) resources. Section 4(f) of the US Department of Transportation Act protects publicly owned parks, recreation areas, wildlife refuges, and historic sites. By law, a Section 4(f) property may be converted to a transportation use only if there is no prudent and feasible alternative and if the project includes all possible planning to minimize harm to the resource. Section 4(f) resources are discussed further in **Appendix B** of this document. **Figure 7** in **Appendix A** presents a map depicting all the Section 4(f) resources in the study area.

Jean Webb Park is maintained by the city. Based on correspondence with city officials (see **Appendix C**), the park is not a significant resource within the city's larger park system; therefore, Section 4(f) is not applicable. FHWA concurred with this determination. However, impacts to the park should still be minimized to the extent possible. The park covers 0.6 acres beneath the RiverLink Bridge. An additional 2.6 acres to the south are also city-owned and have been identified in a planning document as a future park.

Screening against these resources, the Orange Alternative was eliminated because it would lead to greater impacts on Jean Webb Park and the Riverside Industrial Historic District than the Yellow or Green Alternative. The Blue Alternative was eliminated because it leads to greater impacts on the historic district than the Green or Yellow Alternatives. The Purple Alternative was eliminated because it leads to greater impacts on the historic district and the former Hans Rees Tannery property than the Yellow Alternative; the Cyan Alternative was eliminated because it relies on a connection to the Purple Alternative. The Green Alternative was carried forward at the request of the City's Parks and Recreation



Department. **Figure 8 in Appendix A** shows the alternatives that passed the screening against Section 4(f) resources.

C. Recommended Alternative

Based upon consultation with City officials, input from key stakeholders, and technical studies completed for the project, the Yellow/Green Alternative was selected as the Recommended Alternative by the City of Asheville. Generally the Recommended Alternative follows the existing alignment, adding bike lanes, sidewalks, and some on-street parking. A greenway/multi-use path would run between the improved roadway and the river for the length of the project. Intersection improvements at key locations are also included.

Roadway Cross Section and Alignment

The cross section throughout the project varies throughout the project length. Different portions include some of the following elements: a two lane roadway with 11-foot travel lanes, 5-foot bicycle lanes, curb/gutter, a 7-10 foot wide utility strip between the curb and sidewalk, 5-10 foot wide sidewalks or multi-use pathways, and/or 10-foot wide planted median. Opportunities to provide grass swales in the median and utility strip will be considered during final design. Cross section widths on Riverside Drive range from 50-90 feet (excluding the greenway). Along Lyman Street, cross section widths range from 55-105 feet and include the 10-12 foot multi-use path. **Figure 6** shows the minimum and maximum cross sections proposed.

Right of Way and Access Control

The minimum proposed right-of-way width along the Riverside Drive is 50 feet. On Lyman Street, the minimum proposed right-of-way width is 55 feet. Variations in right-of-way width may occur to accommodate intersection improvements or areas where on-street parking is planned.

As a local street that provides access to numerous commercial properties, the proposed facility is not an access controlled facility. Private driveway access will be maintained for existing properties. However, the use of shared or consolidated connections is highly encouraged to reduce conflict points.

Speed Limit

The proposed speed limit along Riverside Drive and Lyman Street within the study area is anticipated to be posted at 25 mph.

Design Speed

The design speed for the facility is 30 mph. Proper horizontal and vertical design criteria will be applied to the project during future design phases, meeting AASHTO and NCDOT standards.

Anticipated Design Exceptions

No design exceptions are anticipated for this project.

Intersections/ Interchanges

No new interchanges are anticipated as part of this project.



The following intersection improvements are recommended based on the capacity analyses for the project, described in the 2012 *Capacity Analysis Report for Wilma Dykeman RiverWay*.

Hill Street / Riverside Drive

- Addition of a southbound left turn lane along Riverside Drive with storage space for 2-3 cars
- Extension of the westbound right turn bay from its existing 60-foot length to 100 feet in length

Craven Street / Riverside Drive

- Addition of a 50-foot long turn bay along Riverside Drive for northbound left turn movements
- Addition of a 50-foot long turn bay along Riverside Drive for southbound left turn movements

Lyman Street / Riverside Drive

- A new roundabout, shifted west of existing location of the Lyman Street/Riverside Drive intersection to address the substandard curve along Lyman Drive near JR Stone Sales and 12 Bones Smokehouse

A one-way and three-way stop controlled T-intersection was evaluated at the Lyman Street/Riverside Drive intersection, but neither performed as well as the roundabout. A roundabout was also considered at the Lyman Street/Amboy Road intersection but is not recommended.

Service Roads

No service roads are included in the proposed project improvements.

Railroad Crossings

There are three at-grade railroad crossings within the study area: along Lyman Street between 5-points and 12 Bones Smokehouse, along Craven Street between Riverside Drive and Roberts Street, and along Riverside Drive north of the railroad bridge.

Along Lyman Street and Craven Street, rail crossings have flashing light signals with automatic gates to restrict crossing traffic. The Recommended Alternative will add sidewalks on both sides of Craven Street to separate pedestrian traffic from vehicles. During final design, the project team will coordinate with Norfolk Southern to coordinate design details at both crossings.

At Riverside Drive, the rail line crosses at a skew angle. Today, there are warning signs and painted markers in the travel lanes. The Recommended Alternative will add sidewalks on both sides of the roadway to separate pedestrian traffic from vehicles. The greenway provides an alternative route from pedestrian and bicycle traffic that does not cross the rail line at this location. During final design, the project team will coordinate with Norfolk Southern to examine the feasibility of additional warning signage and safety measures.



Structures

Several replacement drainage structures have been identified for inclusion as a part of the project, as summarized in **Table 5** and shown in **Figure 9** in **Appendix A**. Excluding structure #1 at the northern end of the project near Hill Street, all existing box culverts and pipe culverts will be replaced.

Table 5: Proposed Drainage Structures

Structure no:	Existing Structure	Proposed Replacement Structure
1	2 x 92.4" x 65" Corrugated Metal Arch	2 x 92.4" x 65" Corrugated Metal Arch
2	4' x 2' Concrete Box Culvert	8' x 4' Concrete Box Culvert
3	Unobtainable	42" Reinforced Concrete Pipe
4	24" Reinforced Concrete Pipe	8' x 4' Concrete Box Culvert
5	5' x 3' Concrete Oval Pipe	2 x 6' x 4' Concrete Box Culvert
6	2 x 10.5' x 7' Corrugated Metal Elliptical	3 x 12' x 8' Box Culvert
7	24" Corrugated Metal Pipe	48" Reinforced Concrete Pipe
8	36" Corrugated Metal Pipe	8' x 4' Concrete Box Culvert

Existing drainage structures are generally undersized and filled with debris. During replacement, channels will be cleaned. Each cross drain on this project discharges to a short ditch or creek channel, which conveys runoff directly to the river; there are no downstream structures. Structure # 6 is expected to be in excess of 20' in length and is therefore large enough to consider a bridge structure during final hydraulic study.

Bicycle Accommodations & Pedestrian Facilities

The Recommended Alternative includes bicycle lanes and sidewalks on either side of Riverside Drive and Lyman Street. Sidewalks will also be constructed along both sides of Craven Street between Riverside Drive and Roberts Street. It also includes a 10-12 foot wide greenway/multi-use path that will run along the western side of the project, paralleling the French Broad River.

Utilities

Construction of the proposed project will likely require some degree of adjustment, relocation, or modification to existing public utilities. No new utility installations are expected at this time. During final design, coordination with affected utility companies, the City of Asheville, and NCDOT will be required.

Landscaping

The landscape and amenity plans developed for the project will follow the River Arts District Design Guidelines that are part of the City of Asheville's adopted *Unified Development Ordinance*. The guidelines strongly encourage the "creative use of landscape materials" and originality. Landscaping shall be provided as required by section 7-11-2 of the *Unified Development Ordinance*.

Species suitable for a river environment shall be specified for required landscaping per *Unified Development Ordinance* Appendix C. In areas distributed by project improvements that are experiencing natural stream bank erosion, riparian vegetation will be planted as a stabilization measure. If this technique is shown to be ineffective, natural rocks will be installed in a manner providing space for



vegetative growth. Other disturbed areas along the project will be reseeded with grass or planted with shrub/groundcover material as appropriate.

The public art goals for the project are to incorporate permanent, temporary, fixed or portable art elements. This may include elements such as patterned paving, tiles, sculpture, paintings, mosaics, or tree wells throughout the project corridor. During final design, further coordination with artists in the River Arts District and art community will be conducted. Public art will be incorporated through City's approved Public Art Policy and implementation guidelines.

Noise Barriers

As discussed in the following chapter, the Recommended Alternative is not expected to create noise impacts within the project limits. No noise barriers are recommended for this project.

Work Zone, Traffic Control, and Construction Phasing

The roadway improvements will include phased construction, similar to the strategy proposed below:

- Phase I will include construction of the section between Hill Street and Craven Street. Existing traffic patterns will be maintained south of Craven Street intersection; therefore, motorists can use Roberts Street to Clingman Avenue to access Patton Avenue and Hillcrest. Access to existing businesses from Riverside Drive will be maintained during Phase I with temporary tie ins. Temporary closures to the railroad spur line will be coordinated with Norfolk Southern.
- Phase II will include widening of Riverside Drive between Craven Street and Lyman Street, including the new location alignment and improvements to the new side street in front of Curve Studios. A new box culvert conveying the tributary to French Broad behind 12 Bones will be completed in Phase II. Rerouting of the traffic patterns to utilize the new Riverside Drive to the north and Lyman Street to 5-Points intersection to access Roberts Street will be encouraged. Widening will be completed while maintaining existing traffic. This will be completed utilizing flagmen in accordance with NCDOT temporary lane closure standard details. At the completion of Phase II, traffic can be shifted to the final traffic patterns north of Lyman Street.
- Phase III will include the construction of the new roundabout at Lyman Street and Riverside Drive, while maintaining traffic along the side street to access north of Lyman Street. Phase III will include widening of Lyman Street south of the roundabout to Amboy Road. Construction of the multi-use path and widening of the southbound lane of the roadway will be completed first, maintaining access to businesses on the east side of Lyman Street. Access to existing businesses will be maintained with driveway ties while widening the northbound lane. The final pavement markings will be installed upon completion of all pavement operations.

All work zone and traffic control will be performed in accordance with NCDOT's Work Zone Advanced Warning Signs and Temporary Lane Closure standards as detailed in Section 1100 of the latest edition of the NCDOT *Roadway Standard Drawings and Standard Specifications for Roads and Structures*, the NCDOT *Guide for Temporary Pedestrian Accommodations*, and the City of Asheville Standards. A



complete and detailed traffic control plan and pavement marking plan will be developed during the final design phase of the project and will be coordinated between NCDOT and the City of Asheville.

D. Cost Estimates

Preliminary cost estimates are presented in **Table 6** below.

Table 6: Preliminary Cost Estimates

Evaluation Factor	Yellow-Green (Recommended)
Roadway Construction Costs	\$10,800,000
Structure Construction Cost	\$2,000,000
Project Total Estimate	\$12,800,000

Note: This cost does not include Right of Way and Utility relocation costs

IV. ENVIRONMENTAL EFFECTS

This chapter describes the existing project area environment then presents impacts for the No Build Alternative and Recommended Alternative.

A. Biotic Resources

Certain plant and animal species are protected by federal and/or state regulations, like the *Federal Endangered Species Act* of 1973, the *North Carolina Endangered Species Act*, and the *North Carolina Plant Protection and Conservation Act* of 1979. In accordance with the *Fish and Wildlife Coordination Act*, the effect of proposed projects on fish and wildlife must be considered. This section discusses wildlife and habitats that can be found in the project area and describes potential effects resulting from the project.

1. Rare and Protected Species

A records search and on-site habitat assessment were conducted during Summer 2010 to determine the likelihood of protected plant and animal species occurring within the limits of the project area. Based on a pedestrian survey of representative wetland and upland habitats within the project area, no federally listed or state-listed threatened or endangered species were observed. Due to the developed industrial land use associated with the project area, the quality of the existing habitat is far below optimal for the listed species that potentially occur in Buncombe County. The quality and ephemeral nature of the aquatic habitat suggest a very low potential for listed fish or mussel species to occur. Three small wetlands exist within the project area but none appear to provide critical habitat for listed animal species or are unique to the region. The *Report of Investigation of Jurisdictional Surface Waters and Protected Animal Species* (December 2010) provides supporting information.

Coordination was undertaken with various resource agencies. In a letter dated September 20, 2010, the North Carolina Natural Heritage Program indicated that no known terrestrial plants, animals, or natural communities are known to occur within the project area; however, historic records document three



aquatic species from the French Broad River. The US Fish and Wildlife Service stated that no listed species or their habitats will be affected by the proposed project and that requirements of Section 7 of the Endangered Species Act have been fulfilled (letter dated October 19, 2010).

2. Terrestrial Communities

Much of the forested habitat in the project area is fragmented and disturbed. Old field habitat is present but also disturbed, with some instances occurring over landfill. Due to the developed industrial land use associated with the project area, the quality of the existing habitat is far below optimal for many terrestrial species. Common avian, reptilian, and mammalian species utilize the project area more than listed wetland-dependent or upland species.

3. Aquatic Communities

As described in **Section IV.B**, intermittent and perennial streams in the project area are few in number and relatively short in length. The quality of the aquatic habitat ranges from low to moderate in these channels. In addition, many of these surface waters are characterized as ephemeral drainage features. Three small wetland areas occur and are comprised of common plant species.

4. Anticipated Effects

The project is anticipated to have no impacts to listed threatened or endangered species. Per the *Fish and Wildlife Coordination Act*, this would result in a “No Effect” finding for species identified as unlikely to occur and a “May Affect, but not likely to Adversely Affect” finding for species identified as having a low potential to occur, summarized in **Table 7**. Due to the limited habitats available in this historically industrial area, any impacts to wildlife or aquatic species found in the area would be minor.

Table 7: Federally Listed Species, Buncombe County

Name		Federal Status ¹	Potential for Habitat Occurrence	Biological Conclusion ²
Appalachian Elktoe	<i>Alasmidonta raveneliana</i>	E	Unlikely (Historic Record)	NE
Tan Riffleshell	<i>Epioblasma florentina walkeri</i>	E	Unlikely (Historic Record)	NE
American Burying Beetle	<i>Nicrophorus americanus</i>	E	Unlikely (Historic Record)	NE
Spotfin Chub	<i>Erimonax monachus</i>	T	Unlikely (Historic Record)	NE
Bog Turtle	<i>Glyptemys muhlenbergii</i>	T (Similar appearance)	Low	MANLAA
Gray Myotis	<i>Myotis grisescens</i>	E	Unlikely	NE
Carolina Northern Flying Squirrel	<i>Glaucomys sabrinus coloratus</i>	E	Low	MANLAA
Eastern Cougar	<i>Puma concolor cougar</i>	E	Unlikely (Historic Record)	NE
Rock Gnome Lichen	<i>Gymnoderma lineare</i>	E	Unlikely	NE



Table 7: Federally Listed Species, Buncombe County

Name		Federal Status ¹	Potential for Habitat Occurrence	Biological Conclusion ²
Spreading Avens	<i>Geum radiatum</i>	E	Unlikely	NE
Bunched Arrowhead	<i>Sagittaria fasciculata</i>	E	Unlikely (Historic Record)	NE
Mountain Sweet Pitcher	<i>Sarracenia jonesii</i>	E	Unlikely (Historic Record)	NE
Blue Ridge Goldenrod	<i>Solidago spithamea</i>	T	Unlikely	NE
Virginai Spiraea	<i>Spiraea virginiana</i>	T	Unlikely	NE

¹ E = Endangered; T = Threatened
² NE = No Effect; MANLAA = May Affect/Not Likely to Adverse Affect

B. Water Resources

Jurisdictional waters of the US (such as ponds, streams, and wetlands) are defined by 33 CFR 328.3(b) and are protected by Section 404 of the Clean Water Act. Professionals conducted field investigations during August 2010 to estimate the location and extent of jurisdictional waterways within the project area. Analyses were performed in accordance with the 1987 *Corps of Engineers Wetlands Delineation Manual* and the 2005 *Identification Methods for the Origins of Intermittent and Perennial Streams* published by the NC Division of Water Quality (DWQ). Additional information on streams and wetlands is provided in the *Investigation of Jurisdictional Surface Waters and Protected Plant and Animal Species Report* (December 2010).

1. Streams and Rivers

In addition to the French Broad River, which flows along the western edge of the project area, 13 surface water features were observed within the project area during the August 2010 field visit. These were identified as either perennial (well-defined channel with continuous water flow), intermittent (well-defined channel that may contain water for only part of the year), or ephemeral (drainage feature that carries stormwater in direct response to precipitation). **Figure 10** in **Appendix A** identifies the location of water features within the project area.

Additional small, manmade ditches and swales were observed within the project area along roadways. These were considered to be components of the local stormwater management system. These are upland cut features, would be affected by stormwater events, and were not shown on USGS topographic mapping. Therefore, it is unlikely that the US Army Corps of Engineers would exert regulatory jurisdiction over these stormwater conveyances.

2. Floodplains & Floodways

The entirety of the project area lies within a low-lying stretch of land along the French Broad River. The Federal Emergency Management Administration (FEMA) has developed floodplain and floodway



boundaries for Bumcombe County. **Figure 10** shows the relationship between the existing roadways and boundaries of the floodway and floodplains.

A floodway is the channel of a river and adjacent land that is reserved to carry floodwaters without increasing the level and extent of flood elevations. Development within the floodway is strictly regulated to prevent increases in upstream flood elevations. In the project area, all of Lyman Street and the southern portion of Riverside Drive falls within the designated floodway.

A floodplain is the area beyond the floodway that would be inundated during a flood event. The 100-year floodplain is the area inundated by a 100-year flood, which is an event that is equaled on average once every 100 years. This equates to a 1% chance that the area will be inundated by a flood event in any given year. The entire project area falls within the 500-year floodplain; the majority falls within the 100-year floodplain as well.

3. Wetlands

Three potentially jurisdictional wetland areas were observed within the project area during the August 2010 field investigations, shown on **Figure 10**.

4. Anticipated Effects

Impacts of the Recommended Alternative on water resources are described below.

Impacts on Streams and Rivers

Impacts to streams will be unavoidable during construction; impacts to jurisdictional and non jurisdictional resources are shown in **Table 8**. In calculating impacts, a 10-foot buffer for construction was assumed along either side of the greenway; a 25-foot buffer was assumed along either side of the roadway. Final details will be determined during the final design phase, in consultation with the appropriate regulatory agencies.

Table 8: Impacts to Streams from Recommended Alternative

Stream ID	Classification	Description	Impact Length
SW1	Intermittent	No Impact	0'
SW2	Perennial	No Impact	0'
SW3	Ephemeral	No Impact	0'
SW4	Ephemeral	No Impact	0'
SW5	Ephemeral	12' crossing greenway + 20' construction buffer (temp)	32'
SW6	Intermittent	12' crossing greenway + 20' construction buffer (temp)	32'
SW7	Ephemeral	74' crossing roadway + 12' crossing greenway + 45' construction buffer (temp)	131'
SW8	Intermittent	12' crossing greenway + 20' construction buffer (temp)	32'



Table 8: Impacts to Streams from Recommended Alternative

Stream ID	Classification	Description	Impact Length
SW9	Perennial	81' crossing roadway + 50' construction buffer (temp)	131'
SW10	Perennial	76' crossing roadway - 64' existing pipe + 50' construction buffer (temp)	62'
SW11	Ephemeral	68' crossing roadway - 41' existing pipe + 50' construction buffer (temp)	77'
SW12	Ephemeral	65' crossing roadway - 41' existing pipe + 50' construction buffer (temp) + 403' parallel within ROW	478'
SW13	Ephemeral	296' parallel within ROW	296'
Total	Perennial		193 linear feet
	Intermittent		64 linear feet
	Ephemeral		1,014 linear feet

Impacts on Floodway & Floodplain

Because the Recommended Alternative primarily widens along the existing Riverside Drive and Lyman Street alignments, much of this alternative would remain within the floodway. For this reason, detailed modeling and floodplain management efforts are expected during future development stages of this project.

All recommended culverts (shown in **Table 5**) are analyzed and sized for the 50 year storm. Because the proposed road and its cross drains will be submerged by the river during this design frequency event, final design efforts may use a lesser design storm frequency than the 50 year storm event that normal policy would require. This will be coordinated as needed with NC DENR and NC Floodplain Management as needed. The length of the proposed culvert structures and the recommended roadway elevation may be adjusted (increased or decreased) to accommodate design floods as determined in the final hydrological study and hydraulic design during final design.

Construction of the Recommended Alternative is not expected to require extensive earthwork, and, therefore, should not result in significant impacts to existing surface drainage or ground water patterns. Some fill will be required toward the south end of the project, retained by walls in order to minimize floodplain and river encroachment. The impact of fill areas will be fully analyzed during the final design process and appropriate mitigation measures and permits will be developed at that time.

The City of Asheville will coordinate with NC Flood Plain Mapping Program (FMP), the designated state agency for administrating Federal Emergency management Agency's (FEMA)



National Flood Insurance Program, to determine the status of the project with regard to applicability of NCDOT's Memorandum of Agreement with FMP, or approval of a Conditional Letter of Map Revision (CLOMR) and subsequent final Letter of Map Revision (LOMR).

Impacts on Wetlands

Wetlands W1, W2, and W3 lie outside the alternative corridor and would not be impacted by the project.

5. Avoidance, Minimization, & Mitigation

The City of Asheville will attempt to avoid and minimize impact to streams and wetlands to the greatest extent possible during project design and construction phases.

Avoidance: Due to the location of streams and surface waters within the project study area, avoidance of all jurisdictional impacts is not possible. Three wetlands identified in the study are lie outside the project corridor, therefore, wetlands are completely avoided by the project.

Minimization: The approved jurisdictional delineation within the project study area will be utilized further minimize surface water impacts when designing the proposed improvements. Placement of fill materials and retaining walls will be designed reduce unnecessary impacts. Utilization of NCDOT best management practices is recommended in an effort to minimize impacts.

Compensatory Mitigation: Compensatory mitigation may be required for unavoidable stream impacts after all practical avoidance and minimization options are utilized. A specific mitigation plan cannot be developed until final design is completed and actual impacts determined. The City of Asheville will coordinate NC DENR to evaluate the need for on-site mitigation during future project phases.

6. Permit Requirements

The following water resources permits are likely to be required.

Section 404 Permits

Section 404 of the Clean Water Act of 1972 authorizes the United States Army Corps of Engineers (USACE) to regulate the discharge of dredged or fill material into waters of the United States including wetlands. Section 404 requires permit applicants to demonstrate with a thorough alternatives analysis that there are no practical alternatives that will avoid adverse impacts to waters of the US. Where impacts are unavoidable, applicants must minimize impacts and provide compensation for impacts.

USACE issues two types of Section 404 Permits: General (Nationwide) and Individual. Individual Permits are required for projects with potential for substantial environmental impacts. The general, or Nationwide, permit program applies to projects with only minor impacts to streams, wetlands and other waters. Nationwide Permit 14 applies to linear transportation projects and allows for a maximum loss of 0.5 acre of non-tidal waters.

As discussed in previous sections, the proposed project corridor contains several jurisdictional wetlands and streams. Most of the streams in the project area are ephemeral, non-jurisdictional streams. It is



likely that the impacts to jurisdictional streams and wetlands will be minimal enough to allow permitting under Nationwide Permit 14; however, USACE holds the final discretion as to what permit will be required to authorize construction.

Section 401 Certifications

Section 401 of the Clean Water Act delegates authority to the states to grant, deny, or condition certification of federal permits to ensure compliance with various provisions of the Act. States consider water quality standards in determining a potential impact on wetland resources. In North Carolina, the 401 Water Quality Certification program is administered by the NC DENR DWQ. The state has issued general certifications that correspond to USACE's Nationwide Permits. DWQ's General Certification 3886 corresponds to Nationwide Permit 14; a joint permit application is submitted to USACE and DWQ for these permits.

Other Permits Required

In addition, construction of the Recommended Alternative will require the following additional permits and certifications:

- Sediment and Erosion Control Plan from NC DENR Division of Energy, Mineral and Land Resources Land Quality Section
- NPDES Stormwater General Permit for Construction Activities from DWQ
- Coordination with City Departments on grading, erosion control, stormwater, utilities, floodplain development, tree removal, etc.
- Stormwater Management Plan
- Coordination with railroad for work in railroad right-of-way
- Coordination with NCDOT for work in NCDOT right-of-way
- FEMA No-Rise Certification

C. Soils

The project area falls within the urban limits of Asheville. The primary soil types are Udorthents-Urban land complexes. Udorthents consist of cut and fill areas where soil and the underlying material have been removed and placed on an adjacent site. Urban land consists of areas where 85% of the surface is covered with buildings, streets, parking lots, or other impervious material; natural soils have been covered, often changing the original landscape and drainage patterns.

D. Farmlands

The Farmlands Protection Policy Act establishes criteria for identifying and considering the effects of federal programs on the conversion of farmland to non-agricultural uses. No farmlands or appropriate soils for farmlands exist within the project area. Historically, the area has been used for primarily industrial purposes. Because the area lies in an urban area, the Farmland Protection Policy Act does not apply and coordination was not undertaken with NRCS.



E. Cultural & Historic Resources

Cultural and historic resources are protected by various federal regulations. Most notably, Section 106 of the National Historic Preservation Act requires federal agencies to consider impacts to historic resources from their actions, and to balance preservation needs with the need for the proposed project. The Section 106 process “seeks to accommodate historic preservation concerns with the need of federal undertakings through consultation ... The goal of the consultation is to identify historic properties potentially affected by the undertaking, assess its effects and seek ways to avoid, minimize, or mitigate any adverse effects on historic properties” (36 CFR 800.1(a)).

The Area of Potential Effects (APE) shown in **Figure 11** in **Appendix A** is part of the historic fabric of the city of Asheville. This area was once comprised of a great mixture of land uses, including industrial, commercial, and residential uses. Dating back to the late 1800s, this area developed with manufacturing, businesses, and recreation taking advantage of the railroad and electric street car access.

During 2010 through 2012, professional archaeologists and architectural historians surveyed the APE to identify historic buildings and any known archaeological features that could be impacted by the proposed project. These findings formed the basis for the Section 106 consultation process and are described in the following subsections.

1. Section 106 Consultation Process

During the fall of 2012, the project team met with the State Historic Preservation Officer (SHPO) and other consulting parties to consult regarding the identification of historic resources and project effects on those resources. The Eastern Band of Cherokee Indians was invited to participate in the consultation process, along with the SHPO and ten local organizations. A meeting was held in Asheville on September 13, 2012 to discuss these issues.

2. Above-Ground Architectural Resources

The APE, shown in **Figure 11**, contains the area likely to experience direct or indirect impacts from the project. The boundary was based on the area’s topography and other visual obstructions. Architectural historians conducted field surveys of the APE during 2010-2012, touring and photographing each structure over 50 years in age. The *Historic Architectural Resources Report* presents the results of this survey, summarized below. The SHPO concurred in these findings in their letter dated July 24, 2012; a copy of the letter is included in **Appendix C**.

- There is one previously listed National Register of Historic Places (NRHP) resource within the APE: the Riverside Industrial Historic District, listed on the NRHP in 2004 under Criteria A and C for its association with commerce, industry, and architecture. Since its listing, three metal buildings from within the Dave Steele complex have been destroyed. As a result of the evaluation completed as part of the River Arts District Transportation Improvement Project, the historic district boundary has been expanded to the south to encompass the two Texas Oil Company buildings along Lyman Street as well.



- The Southern Railroad Bridge, built in 1910, spans the French Broad River, providing a connection from Asheville to Tennessee that is still in use today. The railroad bridge is eligible for listing on the NRHP under Criterion A for its association with the expansion of travel and tourism through Western North Carolina and the connection to the railroads to the west and north. The bridge is also eligible under Criterion C as an excellent example of reinforced continuous girder bridge architecture from the early 20th century.
- The Old Smoky Park Highway Bridge was built in 1948 and today carries westbound I-240 traffic over the river. Construction of this major highway bridge influenced development in West Asheville, particularly its commercial expansion; as a result, the bridge is eligible for NRHP listing under Criterion A. The bridge is also eligible under Criterion C for its design. It represents one of the state highway commission's earliest high profile urban bridge projects in the post WWII period. When built, the bridge was one of the longest applications of continuous design principles by the state bridge unit. The bridge is a historically and technologically significant example of the work of the state highway commission and its bridge unit in the postwar period.
- Two brick Texas Oil Company Buildings along Lyman Street, constructed circa 1917, served as garage and office space for the company. Both buildings are recommended as eligible for listing on the NRHP under Criterion C for their early 20th century industrial architecture. As noted above, these buildings fit within the expanded boundary of the Riverside Industrial Historic District boundary.
- The Hans Rees Tannery located in Asheville in 1898; the location was selected for its proximity to the tanning agent (chestnut oak), its clean water supply, its railroad access, and supply of a labor force. The site included approximately 30 buildings dedicated to the tanning process and employed 250-300 men. A flood in 1916 and fire in 1917 destroyed many of the early structures. The site includes 14 individual structures, several of which feature the tannery's characteristic stepped parapets at the tops of endwalls and internal firewalls. The tannery is eligible for NRHP listing under Criteria A and C for its significant contributions to the tannery industry in Western North Carolina and for its design and construction.
- The Norfolk-Southern Roundhouse is located southeast of the Lyman Street/Amboy Road intersection. The brick roundhouse was constructed in 1926 and features 25 stalls. The structure burned in 1958; today only the northern portion of the structure remains in use. The roundhouse is eligible for NRHP listing under Criterion A for its association with the railroad industry and under Criterion C as an excellent example of an early 20th century railroad facility. It is one of only two known roundhouses remaining within the state.

In addition to the resources described above, 32 properties over 50 years in age within the APE were surveyed but did not meet the criteria for inclusion in the NRHP.

3. Anticipated Effects

The Recommended Alternative (Yellow/Green) will not adversely affect any of the listed or eligible historic properties within the APE. It results in a No Adverse Effect finding for the Southern Railroad Bridge, the Old Smoky Park Highway Bridge, the Riverside Industrial Historic District (including the Texas Oil Company Buildings in the expanded boundary), and the Hans Rees Tannery site. The Recommended



Alternative results in a No Effect finding for the Norfolk-Southern Roundhouse. The signed concurrence form is included in **Appendix C**.

Because portions of two of these eligible historic resources are converted to a transportation use, Section 4(f) laws governing the use of the property apply. After consulting with the SHPO, FHWA has determined that the uses of both the Riverside Industrial Historic District and the Hans Rees Tannery represent De Minimis Section 4(f) uses. Further documentation is provided in **Appendix B**.

4. Avoidance, Minimization, & Mitigation

Measures to minimize harm to historic properties include design modifications (e.g. eliminating on-street parking and median) that lessen impacts, particularly in the vicinity of the Riverside Industrial Historic District. These measures were determined in coordination with stakeholders and the City of Asheville staff.

5. Archaeological Resources

Phase I archaeological investigations were conducted within the project area during 2010. This effort included archival research, pedestrian reconnaissance, and limited shovel testing to assess the potential to encounter archaeological resources in this urbanized, industrial area. Additional information can be found in the *Archaeological Investigations in the River Arts District of the Proposed Wilma Dykeman RiverWay* Report (November 2010).

Shovel testing produced a mix of materials typically including coal and slag several feet in thickness, sometimes including architectural debris as well. Other sites include vast areas formerly serving as a public landfill or areas where architectural debris from other sites was subsequently deposited. Investigations suggest that the project area includes, in addition to extant historic architecture, the potential for archaeological remains directly associated with industrial Asheville dating from circa 1880 to 1950. However, no in situ deposits referable to late 19th or early 20th century Asheville were recognized in recent shovel tests, nor were any prehistoric materials recovered. Tests indicate that should any prehistoric or early historic contexts survive, these deposits are deeply buried beneath modern overburden. Should construction of the project require excavation to depths greater than one meter, an archaeologist should be employed to monitor these excavations.

F. Community Resources

The existing project area falls within the River Arts District of Asheville. The District is largely industrial, home to operating industries, local cafes, and artist studios operating in revitalized industrial warehouses. Due to the relatively low cost of real estate and low rent rates in this area, other uses have gravitated to this area, including small offices, some retail shops including antiques stores, a music hall venue, event spaces, a brewery and some services. **Figure 12 in Appendix A** identifies key community resources located within and adjacent to the River Arts District.

Beyond a few artist studios with live-in loft spaces, there is no residential development within the project area itself. No residences will be displaced by the Recommended Alternative.



There is one park within the project area: Jean Webb Park, a city-owned park located north of the RiverLink Bridge. The park is an open greenspace with picnic tables and river access; it covers 0.6 acres along the French Broad River today. A 2.6-acre parcel immediately south of the RiverLink Bridge is owned by the city and designated as a future park. Parking is provided beneath the bridge today. The Recommended Alternative will require additional right-of-way within the park. However, the City has indicated that Jean Webb Park is not a significant resource within their recreational system and that the Recommended Alternative is consistent with their future plans for the resource.

There is one school located within the project area. The French Broad River Academy is a small, private middle school for boys located within the project area. There are no churches within the project area. The nearest medical facility is Mission Hospital, located off Biltmore Avenue east of the River Arts District. The nearest cemetery, Riverside Cemetery, is located north of the project area. No fire stations, police stations, post offices, libraries, or other public facilities are located in close proximity to the project area. The project will not directly impact any of these community resources.

1. Anticipated Effects

Four Businesses will be directly impacted by the proposed project and would be relocated. Relocated businesses will experience a short-term loss in sales and patrons while they are being relocated. If the new location is not easily accessible or desirable to customers, it may affect future sales. The following businesses will be subject to relocations are:

- Cinder block building, adjacent to Riverside Studios at 174 W Haywood Street, leased out as studio space
- T&T Enterprises, Lumber Yard/Sawmill at 300 Riverside Dr
- Warren Andrews & Sons Planters No. 1 Tobacco Warehouse at 221 Riverside Dr
- 12 Bones Restaurant at 5 Riverside Dr

Comparable properties and rental spaces are available in the vicinity of the River Arts District, if the business owners would like to relocate their businesses to an alternate location. The City and a variety of organizations are available to assist in this process. Right-of-way acquisition will be governed by applicable state and federal regulations. The City will provide relocation assistance through the Uniform Relocation Assistance and Real Property Acquisition Policies Act. Per NCDOT policy, comparable replacement must be available for relocatees prior to construction of state/federally funded projects. NCDOT has three programs to minimize the inconvenience of relocation: relocation assistance, relocation moving payment, and relocation replacement payments or rent supplies.

Except for Jean Webb Park, the project will not directly impact any parks, schools, churches, or other community resources. The Asheville Parks Department has indicated that the Green Alternative represents an opportunity to improve Jean Webb Park. While the Recommended Alternative results in the use of 0.37 acres of the city-owned parcel planned for a future park, this park does not represent a significant component of the city's park system; therefore, Section 4(f) is not applicable.



2. Environmental Justice

Executive Order 12898 requires all federal agencies to “make achieving environmental justice a part of its mission by identifying and addressing, as appropriate, disproportionately high adverse human health or environmental effects of its programs, policies, and activities on minority and low-income populations.” Aside from a few loft spaces in artist studios, there are no permanent residents within the project area. Based on 2000 and 2010 Census data, minority concentrations are considerably higher within the Census tracts and block groups in the vicinity of the project area than within the state or county. An estimated 31% of the population in these block groups is living in poverty, compared to 11% in Buncombe County.

While minority and low income etc populations are present in the vicinity of the project, no notably adverse community impacts are anticipated with this project; thus, impacts to minority and low income populations do not appear to be disproportionately high and adverse and no denial of benefit is expected.

Limited English Proficiency

Individuals with limited English proficiency (that is, individuals who do not speak English as their primary language and who have a limited ability to read, write, speak, or understand English) live in the vicinity of the project. Based on American Community Survey data (2006-2010), an estimated 0.6% of the population 18 years of age and older within the three study area block groups speaks English less than very well. This compares to a county wide rate of 5% and a statewide rate of 6%. Because there are no language groups in which more than 5% of the population or 1,000 persons speak English less than very well, this demographic assessment does not indicate the presence of Limited English proficiency (LEP) groups that exceed the Department of Justice’s *Safe Harbor Act* threshold.

G. Land Use

Historically, the flat, narrow strip of land immediately east of the French Broad River developed with primarily industrial uses. Industries within the district began a slow period of decline beginning during the Depression of the 1930s and running through the post-World War II era. Today, a few industrial uses remain in operation, concentrated north of the I-240 bridge and south of the Riverside Drive and Lyman Street intersection. A majority of the riverfront area has been converted to commercial uses. Redevelopment is occurring within the River Arts District, transitioning the district into the increasingly popular destination that it is today. Today, more than 180 artists have created galleries and studios in buildings that once housed mills, warehouses, and manufacturing operations. The River Arts District Artist Association promotes arts and commerce in the district with regularly scheduled Studio Strolls which draw local residents and tourists to the district. To complete the fabric of this district, restaurants, breweries, housing, office spaces, and performance facilities have joined the mix to create a vibrant destination built on the cultural of arts and providing a new attraction for visitors to the region.

All new development in the River Arts District is regulated by the City of Asheville *Unified Development Ordinance* (UDO). According to the UDO, most of the study area is in the River District zoning district, which allows a wide variety of residential and non-residential uses that can be mixed vertically. In addition, all development proposals are subject to review by the River District Design Review Board.



Future Land Use Guidelines

As discussed in **Chapter I**, a variety of recent planning documents describe elements of the future vision for the River Arts District and Asheville, focused on development opportunities to create an economically vibrant, mixed-use area that would improve the quality of life in the study area as well as Asheville. While there are opportunities for in-fill and new development within the vicinity of the project, a range of constraints limit opportunities within the River Arts District. The vast majority of the area between the rail line and the river lies within the 100-year floodplain, including 11.2 acres within the floodway. As discussed in **Section V.L**, 15 parcels within the district have existing, pending, or potential land use restrictions and 21 parcels contain potential soil or groundwater contamination.

1. Anticipated Effects

An expanded series of parks are proposed along the riverfront north of Lyman Street. These open spaces minimize floodway impacts and increase linkages to the natural environment, consistent with the visions outlined in planning documents. The remaining portions of the district are slated for conversion to mixed-use spaces. This is consistent with the ongoing redevelopment trend evident along Depot Street and Clingman Avenue, as historic industrial structures are being rehabilitated and repurposed to serve as studios, cafes, shops, and more. While the recent economic recession has stalled this process, the long term vision stands. Consistent with the conclusions of recent planning efforts, development patterns are expected to evolve with emerging nodes of activity linked by a well-connected network of streets, greenways, and blueways along the river.

On its own, the project will not increase demand for redevelopment or appreciably accelerate existing redevelopment trends in the district. Rather, it facilitates ongoing efforts to revitalize the riverfront through improved safety and enhanced pedestrian and bicycle access. Right-of-way acquisition for the Recommended Alternative falls largely along existing corridors and within the floodplain, minimizing impacts on less constrained opportunity zones further from the floodway.

H. Visual Environment

The River Arts District lies on a relatively flat, narrow strip of land on the eastern bank of the French Broad River. The terrain and vegetation limit views to the east and west. The visual character of the area is gritty, with a mix of aged industrial structures, natural vegetated areas along the riverfront, and a series of rail lines running north-south. Recent redevelopment efforts have been concentrated along Depot Street. A number of historic structures have been refurbished to serve as cafes and artists' studios. Residential areas, parks, and some commercial developments are located on top of the ridges to the east and west; views to and from these areas are largely screened by trees.

The Recommended Alternative is largely located on existing alignment. The improvements are not anticipated to introduce additional structures, traffic, or other visually discordant elements that would noticeably impact the visual setting of the area. The greenway/multi-use path will provide access to new views of natural areas and parklands along the river. As with all transportation projects in Asheville, a portion of the construction budget will be used to incorporate public art, which should be designed to be consistent with the character of the River Arts District.



I. Economics

For decades, arts and culture have played an important role in the economy and lifestyle of Asheville, enhancing the quality of life for area residents and playing a significant role in attracting visitors and dollars to the region. The River Arts District brings together the community's history, environment, and arts and crafts culture to attract residents and visitors alike to this unique community.

The River Arts District Transportation Project has been planned and designed to help support the transportation needs of the businesses and people within the district. It is consistent with the unique character and environmental quality that is important in this region. While it is not anticipated to attract new trips to the district, it will facilitate safer movements for the motorists, cyclists, and pedestrians traveling within and through the area. The project has also been planned and designed to enhance the visitor experience in the River Arts District by improving multi-modal access through and within the district.

Four business relocations will result from implementation of the Recommended Alternative. While this may affect economics for individual businesses, comparable properties and rental spaces are available in the vicinity of the River Arts District, if the business owners would like to relocate their businesses to an alternate location. The City and a variety of organizations are available to assist in this process. Overall, implementation of the Recommended Alternative will have minimal impact on the city's economy.

J. Noise

Construction of the project will not increase traffic volumes when compared to the future no build scenario. Neither the operating speed nor vehicle mix is anticipated to vary. Therefore, permanent noise levels due to traffic will not increase.

The major construction activities for this project are expected to include earth removal, hauling, grading, and paving. Temporary and localized construction noise impacts will likely occur as a result of these activities. Temporary speech interference for passersby and individuals living or working near the project can be expected. Noise levels in the project area will be increased during construction.

K. Air Quality

Buncombe County meets the National Ambient Air Quality Standards established by the EPA for all criteria pollutants. The proposed project is located in an attainment area; therefore, 40 CFR Parts 51 and 93 are not applicable. This project is not anticipated to create any adverse effects on the air quality of this attainment area.

For both the No Build and the Build Alternatives, the amount of MSATs emitted would be proportional to the vehicle miles traveled or VMT, assuming that other variables such as fleet mix are the same. The VMT along this roadway is not expected to change. Motorists will have an improved and more efficient connection between Hill Street and Amboy Road. No significant MSAT impacts are anticipated from this project.



L. Hazardous Materials Sites

During 2010, reviews of readily-available historical information, interviews with local and regulatory sources, and field reconnaissance were conducted to develop an understanding of potential hazardous substances or petroleum products within the project area. The *Report of Evaluation of Environmental Conditions* (October 2010) contains a technical report that presents the detailed findings of this investigation.

Properties in the project area shown in **Figure 13** in **Appendix A** were identified to have known or potential contamination resulting from current or historical operations, as well as properties where identified contaminants have been remediated or received regulatory closure. Fifteen properties/parcels have existing, pending, or potential land use restrictions. Twenty-one properties/parcels contain potential soil or groundwater contamination.

The Recommended Alternative requires additional right-of-way from sites 3-10, 12-13, 17-18, 20-21, 23-25, 42, and 46. Generally, there are strip takings parallel to the existing roadways. Of these sites, 17, 20-21, 23-24 and 25 have in-place agreements that restrict property usage. Details will be worked out during the final design phase of work to reduce impacts. During construction, it is anticipated that low monetary and scheduling impacts will result from these sites.

M. Indirect & Cumulative Effects

Indirect effects are caused by the action and are later in time or farther removed in distance, but are still reasonably foreseeable. Indirect effects may include growth inducing effects and other impacts related to induced changes in the pattern of land use, population density, or growth rate and related effects on air, water, and other natural systems (40 CFR 1508.8).

The potential for indirect effects from the project is limited by the scale of the project and integrated planning efforts undertaken by the City:

- The project does not provide new vehicular capacity and generally follows existing right-of-way. The project is needed to accommodate ongoing growth and redevelopment in the River Arts District; it is not a driving force spurring this trend. Though the project will provide improved safety and enhanced multimodal connections, its construction is not anticipated to induce new trips to or through the area.
- Further, development regulations guide growth in the area, to ensure new construction and redevelopment efforts are consistent with established visions for the area's future. The City's 2008 *Unified Development Ordinance* includes the project area; it requires development activities to maintain a 30-foot undisturbed buffer around all perennial and intermittent streams. Further restrictions protect the floodway, which covers a large portion of the study area.

The potential effects from Transportation Impact Causing Activities (TICAs) are outlined in **Table 9**.



Table 9: Indirect effects from TICAs

TICA	Resulting Effects from Project
Travel Times	Despite minor improvements for some turning movements at key intersections, the project will not result in notable travel time savings.
Travel Patterns	The Recommended Alternative includes a short segment on new alignment near Jean Webb Park. However, it will have negligible effects on existing travel patterns.
Property Access	The Recommended Alternative will not impact vehicular access patterns for individual properties. It will increase accessibility for cyclists and pedestrians through an improved network of sidewalks and greenways.
Property Exposure	The Recommended Alternative will not induce new vehicular trips to or through the District.
Creation of a Land Use or Transportation Node	The Recommended Alternative is intended to facilitate ongoing redevelopment in the District but will not open new areas for travel or growth.

The Council on Environmental Quality defines cumulative effects as the impact on the environment which results from the incremental impact of the action when added to other past, present, and reasonably foreseeable future actions regardless of what who undertakes such other actions (40 CFR 1508.7). As indirect and direct impacts associated with this project have been avoided, minimized, or mitigated according to the NEPA process, the potential for adverse cumulative impacts to the human and natural environment is also lessened. This project will not notably contribute to cumulative impacts to environmental resources in the vicinity.

V. COMMENTS AND COORDINATION

This chapter describes the extensive public, agency, and other outreach efforts undertaken to coordinate with the local community and other stakeholders.

As part of the project, a *Public & Stakeholder Involvement Plan* was developed to identify key stakeholders and ensure that a wide range of interested parties would have opportunities to provide input during the NEPA process. This led to a community outreach and public involvement process that was tailored to meet the needs of the engaged Asheville community, building upon previous work efforts. The project team worked closely with the City of Asheville’s Public Information Officer, the neighborhood/volunteer coordinator, and the City’s Project Manager to ensure that information was distributed to the public and public meeting notices were well advertised. Meeting locations were carefully selected in the vicinity of the project area.

A. Community Informational Workshops

Three public meetings have been held over the duration of the project.



Public Kickoff Event

The project held a public kickoff event on June 22, 2010 at the Seven Star Factory on Lyman Street. The meeting was well attended by city staff, local officials, local media representatives, and other citizens.

January 2011 Public Meeting

A public meeting was held at AB Tech on January 22, 2011 to explain the project to interested parties and to obtain public input. A total of 59 individuals attended. A presentation provided an overview of the project: a brief overview of the project's propose and need, existing resources within the study area, alignment options, and public involvement opportunities. Additional stations were set up to share information about historic properties, an overview of the NEPA process, study area alternatives, and typical cross sections.

A summary of the public meeting is included in **Appendix D**. Generally, citizens were excited about the opportunity of the proposed project and the proposed changes that the project would bring to the River Arts District. Input received included the following.

- Several citizens voiced an opinion on their preferred alternative. The purple alternative was preferred by majority of citizens. Majority of the meeting attendees suggested that the purple alternative offered the most opportunities for revitalization in the study area. However, some recommend keeping existing alignment. Other suggestions included a one way pair with a road in front and a road in back of the Riverview Station building. Some comments suggested considering various hybrids of the color coded alternatives.
- The project should minimize historic property impacts, including resources that are included in the Asheville Riverside Industrial Historic District.
- Citizens supported properly sized and continuous sidewalks, marked crosswalks, and consistent and clear signage. The comments also suggested the need for a better pedestrian environment separate from vehicles and for improved river access.
- Some attendees identified a need for bike/pedestrian access to the river from the RiverLink bridge, as well as a separate bike/pedestrian bridge.
- Public comments supported the multimodal transportation need and the Complete Street design for the corridor. Implementation of greenway and safer bike/pedestrian transportation improvements were suggested as immediate improvements.

September 2011 Public Workshop

The second informational workshop was held at the Asheville Area Habitat for Humanity on September 29, 2011. The project team presented the study process to date and gathered feedback from members of the public on potential alternatives. Stations were set up around the room to present information about the project's history, environmental features in the project area, the project purpose and need, and public art. Mapping showing each alternative plus a 3-D model were also displayed. A total of 47 individuals attended the second Public Meeting. A summary of the meeting is included in **Appendix D**.

At each station, citizens were invited to speak with members of the project team and also were asked to fill out a comment sheet. Between the meeting and following 30-day comment period, the team



received alternative surveys from 25 individuals, feedback forms from 16 individuals, preferred alternative sketch maps from 22 individuals, purpose/need/goals matrices worksheets from 11 individuals, plus a petition. Respondents unanimously agreed that Riverside Drive and/or Lyman Drive need to be improved and over 80% of respondents identified pedestrian safety as a top need. Responses indicated pedestrian safety was the top mobility need in the project area, followed by the lack of sidewalks. Several people indicated that existing safety problems will be compounded as the area develops. Responses were well distributed when asked what types of transportation improvements were needed; sidewalks and bike lanes were the top two items identified.

Newsletters

Four newsletters were distributed to provide public updates.

- Newsletter 1 was mailed in January 2010. The purpose of the first newsletter was to give public an update on the project purpose and need, goals, and project description. Contact information for the project was also included in the newsletter.
- The second newsletter was distributed in January 2011 electronically to the mailing list of approximately 200 people developed for the project. The email distribution also included attendees from the first public meeting.
- Third newsletter was distributed during the second public meeting in September 2011. The purpose of the third newsletter was to give the public an update on the project, alternatives, cross sections, and the 3D model developed for the project.
- The final newsletter was distributed in December 2012 at the public workshop. It provided an overview of content in this CE including the project Purpose and Need Statement, an overview of alternatives considered, a description of factors that led to the selection of the Recommended Alternative, and a summary table of resulting impacts on the environment.

Copies of the newsletters are located in **Appendix D**.

B. Public Workshop for CE

A final public workshop was held at the Dr. Wesley Grant Southside Community Center on December 6, 2012 to explain the project to the public and to obtain public input. The workshop was advertised on local media and also was publicized as public notice delivered to study area residents and business owners. The meeting notices were also hand delivered to the study area art studios. An estimated 98 people attended the meeting, including members of the project team. Concern and comments raised by participants came in the form of questions, suggestions, and discussion during the public workshop and comments received via email to the project team during the comment period. Generally, comments included the following.

- A number of organizations, local businesses, and citizens expressed their support for the Recommended Alternative.
- The most common concerns expressed related to the impacts on RAD businesses, specifically 12 Bones.



- Other responses identified suggestions to provide additional parking, improve amenities for pedestrians and cyclists, facilitate truck movements, adjust the alignment, reconfigure sidewalks, etc.

A summary of the meeting and a detailed overview of public comments is included in **Appendix D**.

C. NEPA/404 Merger Process

As the project is being sponsored by the city rather than the DOT, the Interagency 404 Merger Process was not initiated for this project. However, resource agencies were invited to provide scoping comments in 2010. Copies of these agency letters are presented in **Appendix C**.

On August 10, 2011 the project team conducted a webinar on the project for representatives of the Environmental Protection Agency (EPA), FHWA, and the Federal Transit Administration (FTA). The webinar covered the background and location of the project. The emphasis was that the project should be relevant and useful to the community. It is hoped that Wilma Dykeman RiverWay will become an example of successful design for future projects. As a result of the meeting, the agencies planned to cooperate to compare performance measures between agencies and to refine the phrasing the project's purpose and need. This webinar was followed by work session with NCDOT, EPA, FHWA, and the FTA in Asheville on September 13, 2011. At the meeting, the agencies refined the project purpose, need, and goals. They also explored the livability characteristics of the project, which make this effort a potential example for sustainability projects in other locations.

D. Other Coordination

In addition to the public and agency outreach efforts described above, the project team met with a range of committees, stakeholder groups, city officials, and other interested citizens over the duration of the project.

A Citizens Advisory Committee was established that included stakeholders from the River Arts District, business owners, and other local commissions. A Steering Committee was established that included NCDOT, the City of Asheville, and RiverLink. Both committees toured the study area, met several times throughout the project, and received regular updates. Committees provided input relevant to the definition of purpose and need, the development of alternatives, and coordination with other city projects. Input from these groups was taken into consideration during the development and refinement of alternatives.

Throughout Fall 2010, a series of stakeholder interviews were conducted to reach out to community members about their expectations and concerns regarding the project. Specific emphases included bicycle issues, neighborhood connectivity, the business community, artist expectations, commuter problems, and past planning visions.

On July 18, 2012, the project team met with representatives from the Norfolk Southern Corporation to discuss the project and tour the project area to see how railroad crossings could be improved. The tour included the Lyman Street crossing near the RiverLink Office, the Craven Street crossing near White



Duck Tacos, the Riverside Drive crossing near Hill Street, and Riverside Drive at the Railroad Bridge. Following the tour, examined precedent images for pedestrian safety devices to see if they could be applicable to RAD. The group also focused improving safety for pedestrian crossings at Lyman Street. The City and the Railroad would like to add elements like benches, gates, and trash bins to the plans. The Railroad would also like to implement more signage to avoid people trespassing on railroad grounds. The Project Team will work further with the City and the Railroad during final design.

Project team members also provided updates at a number of regularly scheduled meetings for other community groups. This included RiverLink, the Asheville Greenway Commission, the Sierra Club, the Asheville Area Riverfront Redevelopment Commission, the city's Bicycle and Pedestrian Task Force, and more.

VI. CONCLUSIONS

This chapter describes the extensive public, agency, and other outreach efforts undertaken to coordinate with the local community and other stakeholders.

Based on the findings of project technical studies, coordination with the public, and input from regulatory agencies, there is no indication that implementing the proposed improvements would have an overall significant adverse impact on the environment. The Recommended Alternative (Yellow/Green) generally widens along the existing alignments of Riverside Drive and Lyman Street to provide medians, on-street parking, and improved sidewalk connections. Just south of the RiverLink Bridge, the Yellow/Green Alternative swings west towards the river to correct substandard geometrics along existing Lyman Drive.

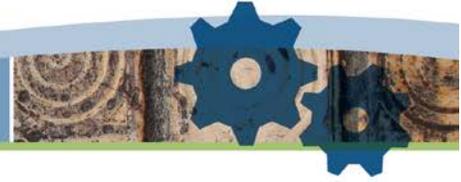
Measures to minimize and avoid impacts to the human and natural environment have been incorporated throughout the planning stages and the development of preliminary design for the Recommended Alternative. Due to its limited scope and lack of substantial environmental impacts, this project is considered to be a Categorical Exclusion.

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APPENDIX A

Figures

RIVER ARTS DISTRICT TRANSPORTATION PROJECT

A Section of the Wilma Dykeman Riverway



Asheville, Buncombe County, North Carolina

STUDY AREA

Legend

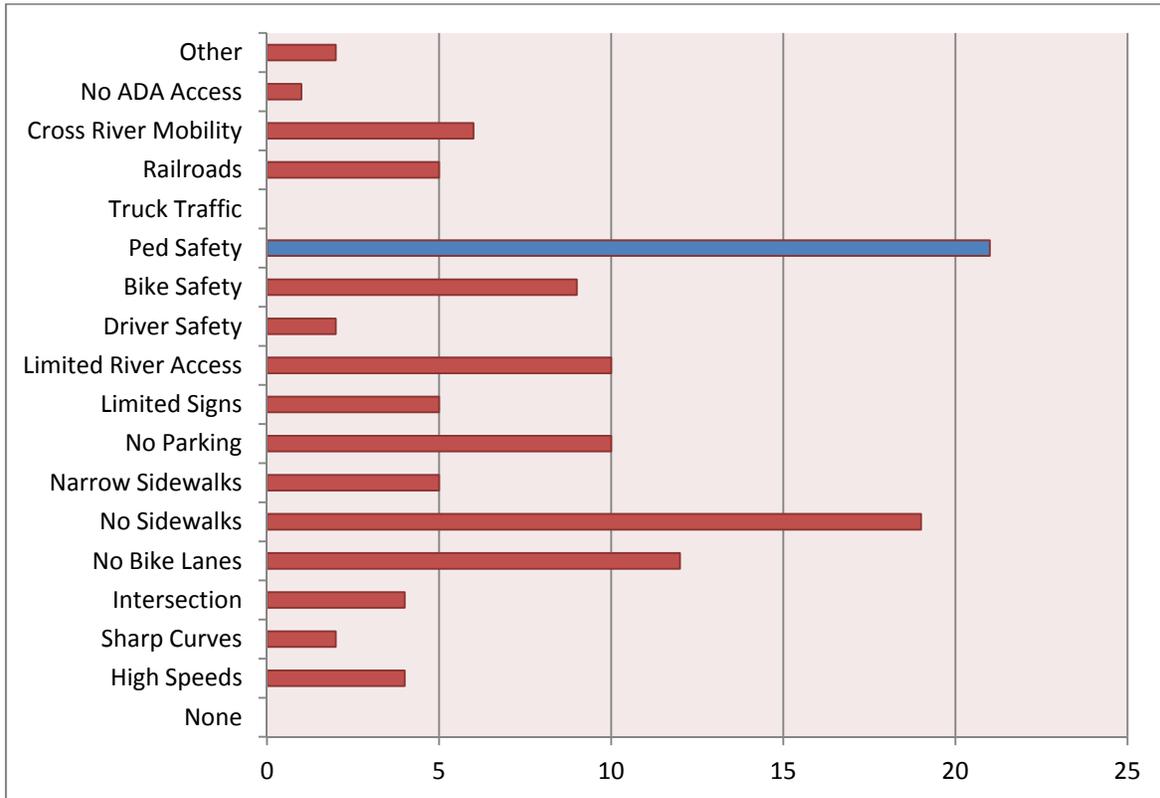
- Study Area

0 125 250 500 750 1,000 Feet

FIGURE 1

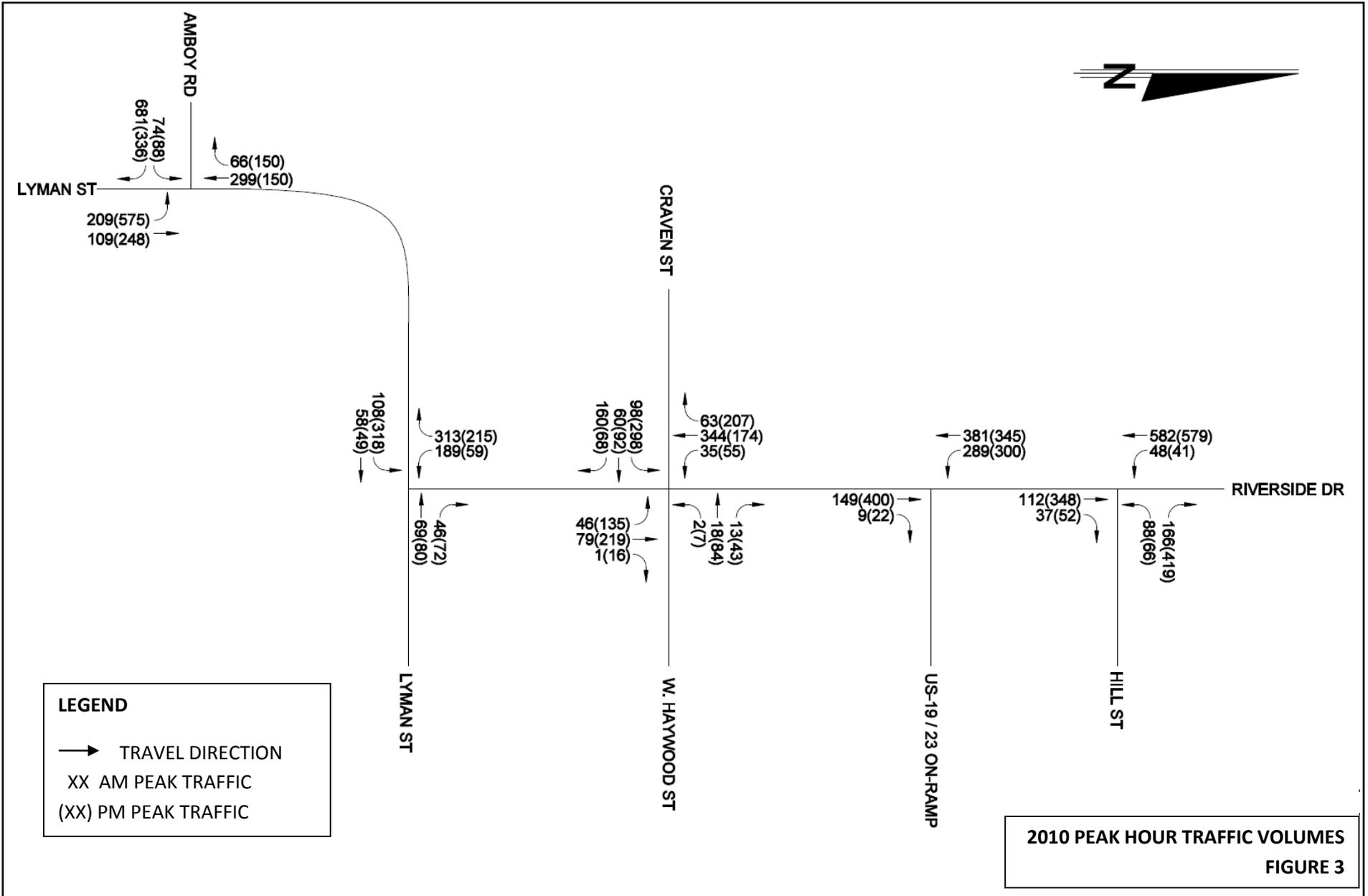


Figure 2: Survey Results identifying the top 5 Mobility Issues in the River Arts District



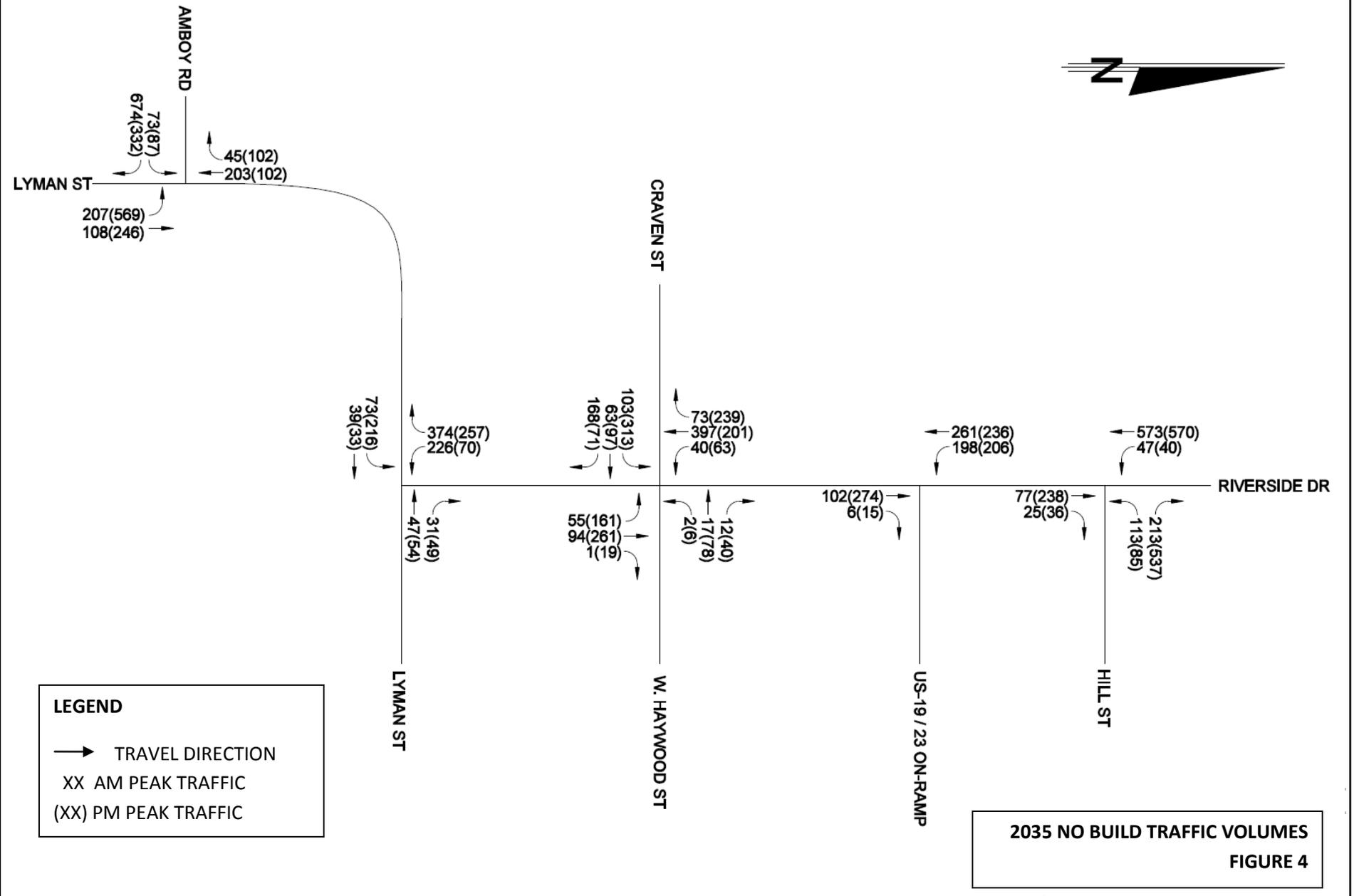
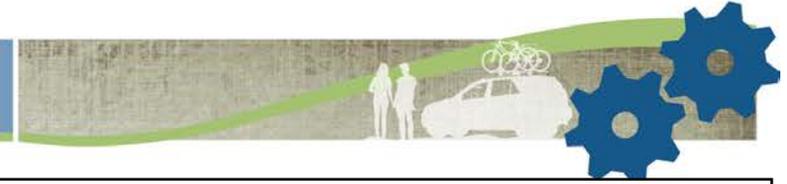
RIVER ARTS DISTRICT TRANSPORTATION PROJECT

A Section of the Wilma Dykeman Riverway



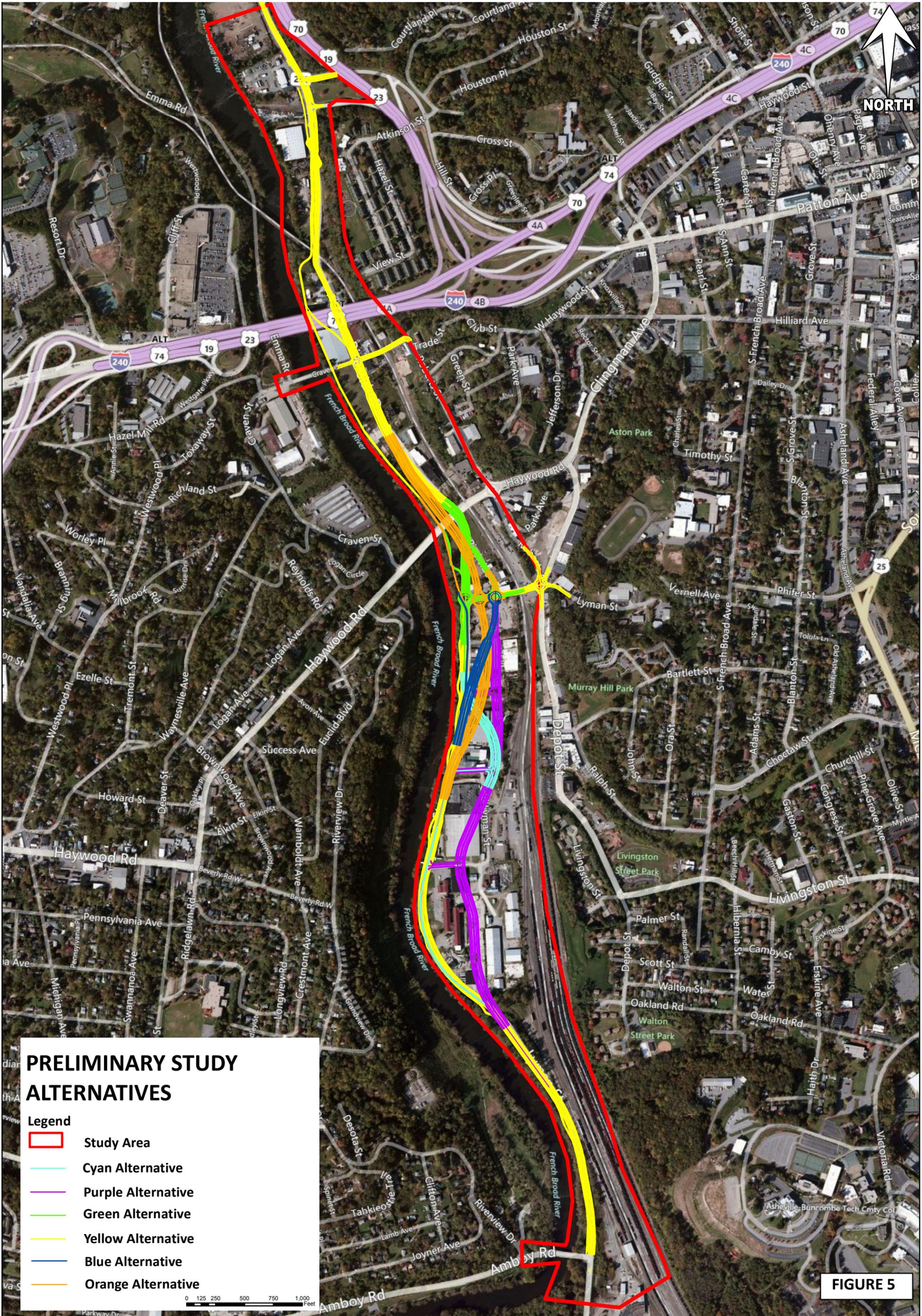
RIVER ARTS DISTRICT TRANSPORTATION PROJECT

A Section of the Wilma Dykeman Riverway



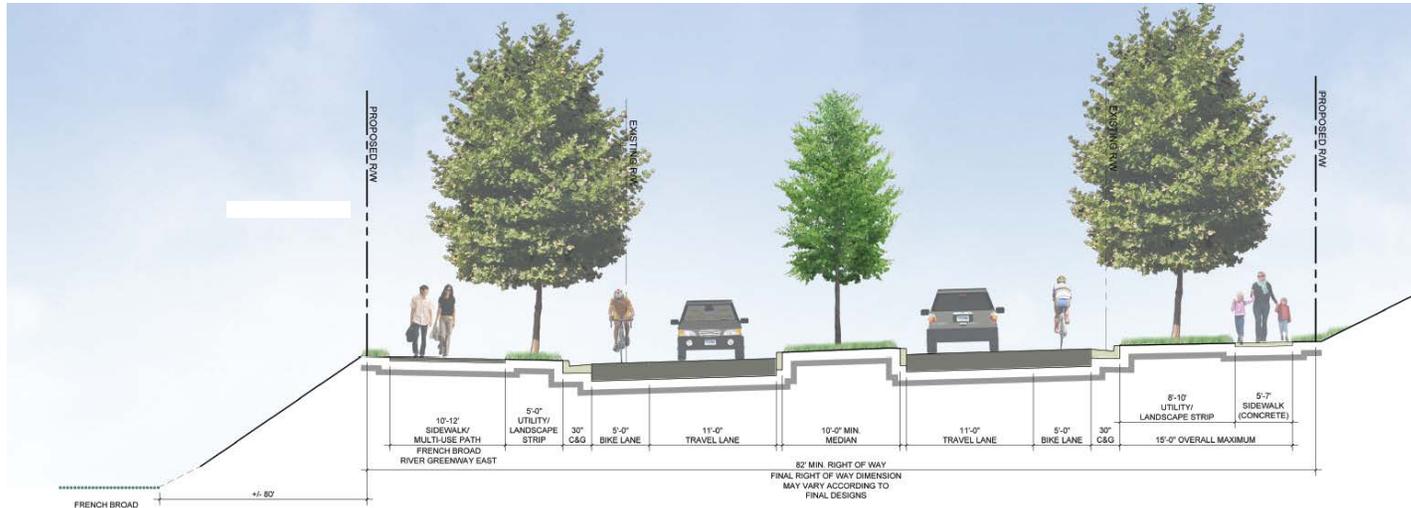
RIVER ARTS DISTRICT TRANSPORTATION PROJECT

A Section of the Wilma Dykeman Riverway



RIVER ARTS DISTRICT TRANSPORTATION PROJECT

A Section of the Wilma Dykeman Riverway



**TYPICAL CROSS SECTIONS
FIGURE 6**

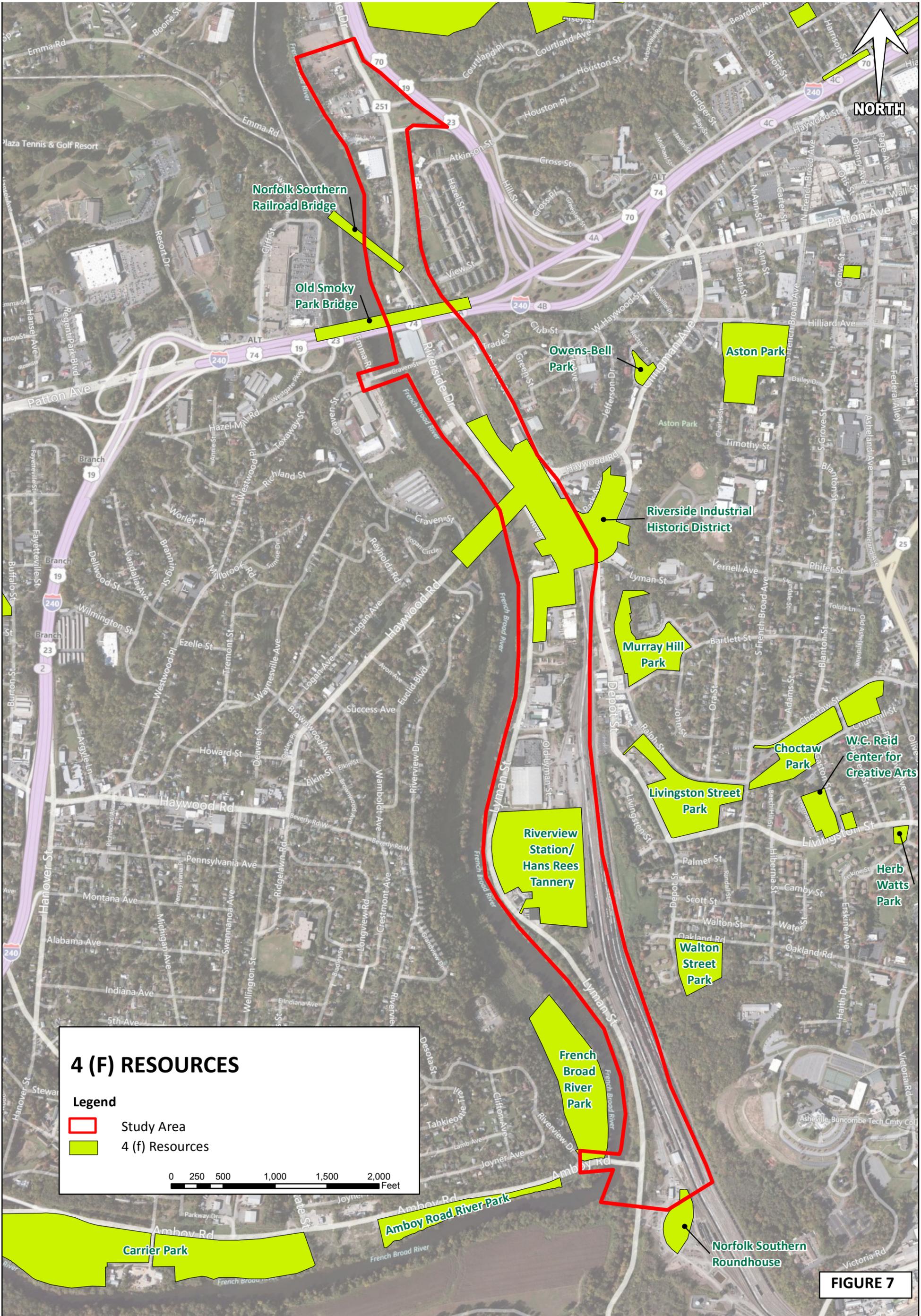
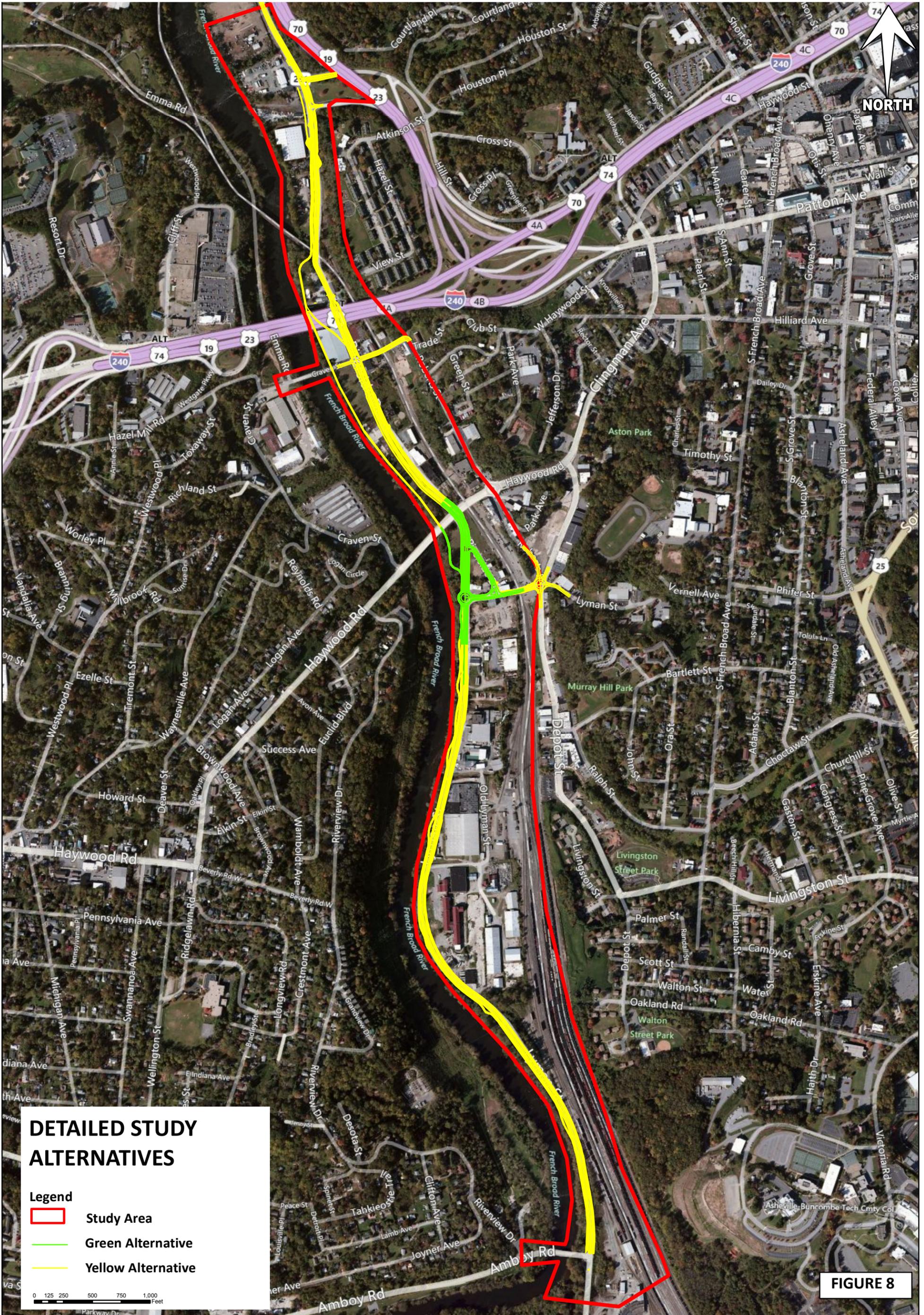
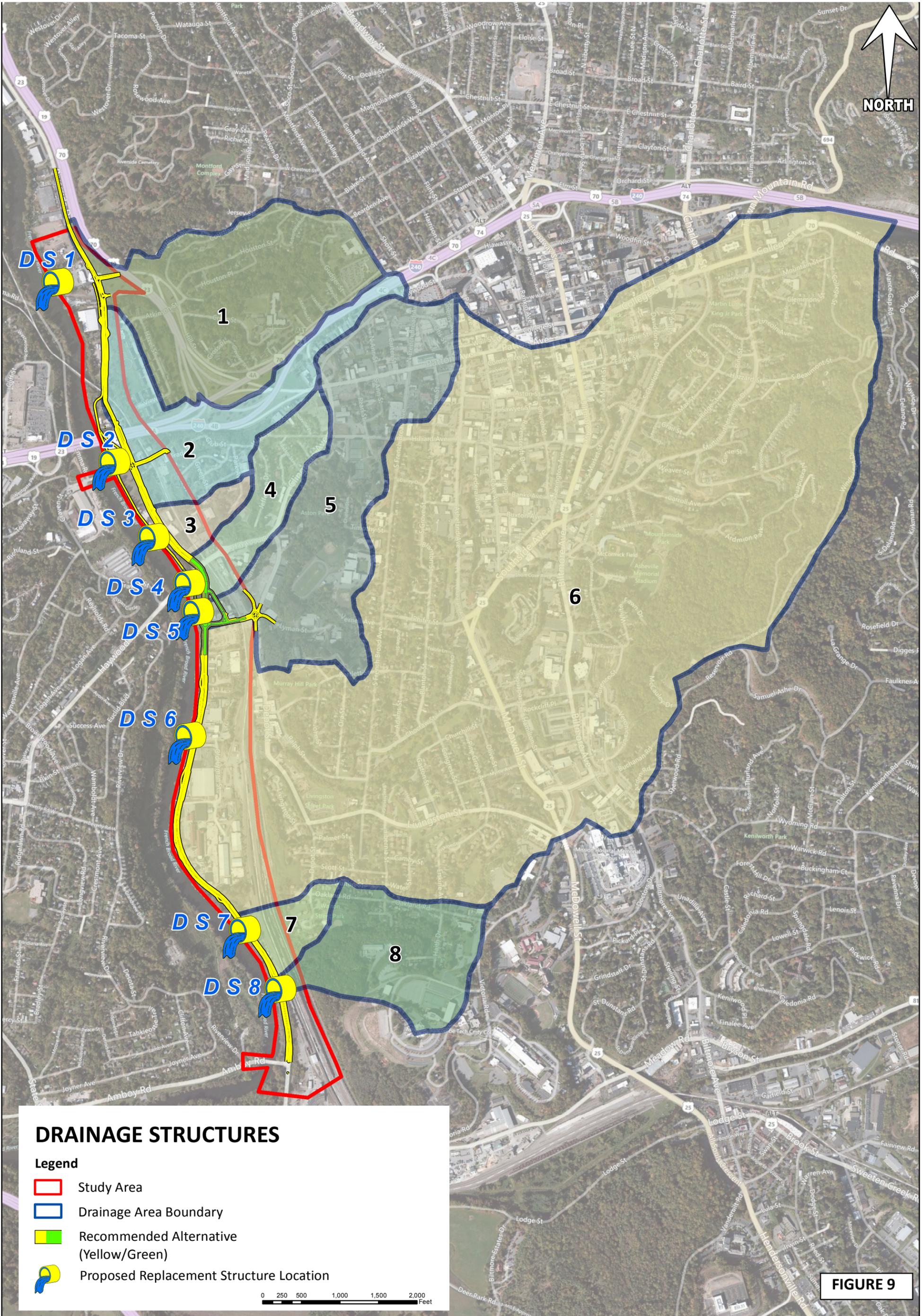


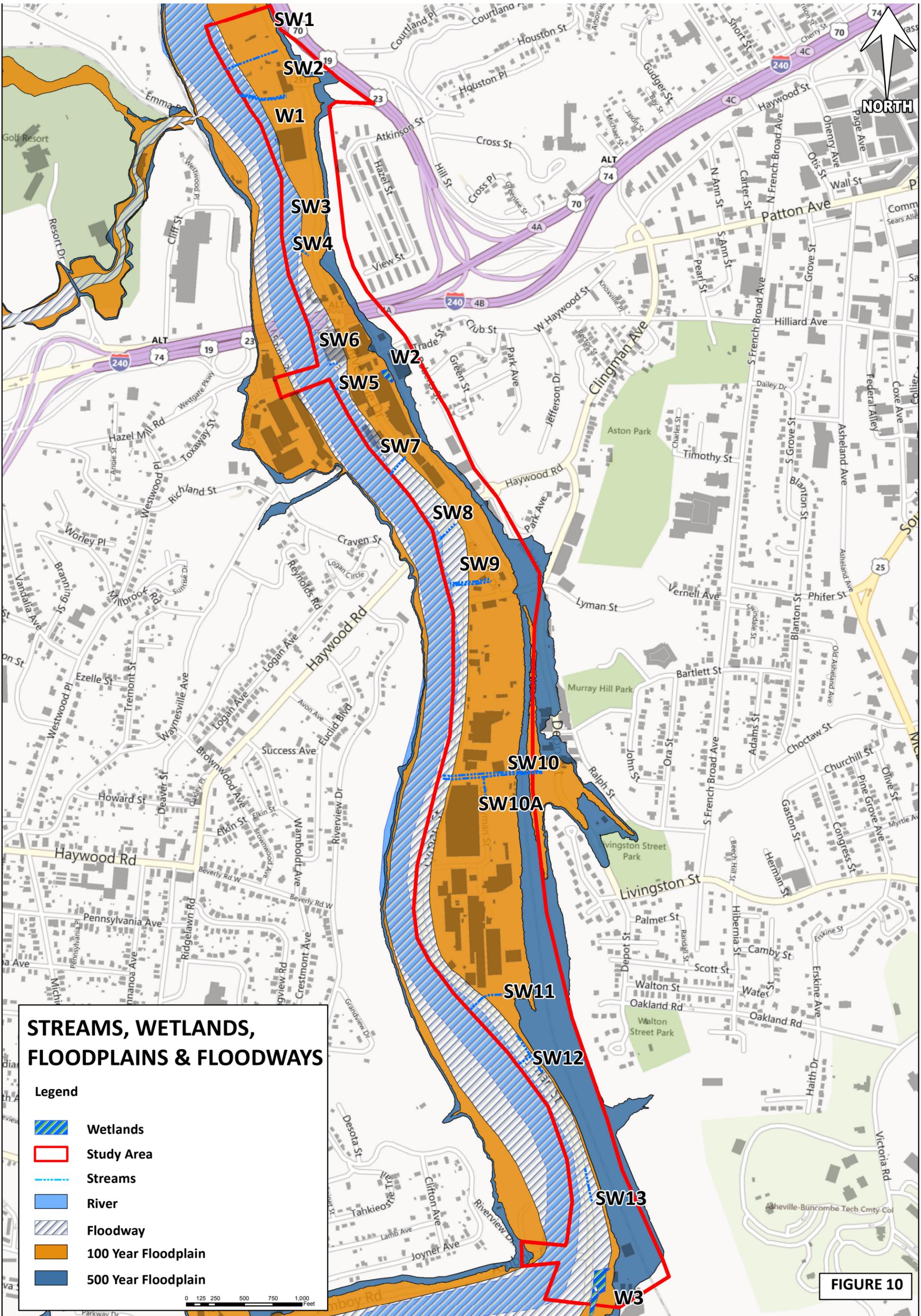
FIGURE 7

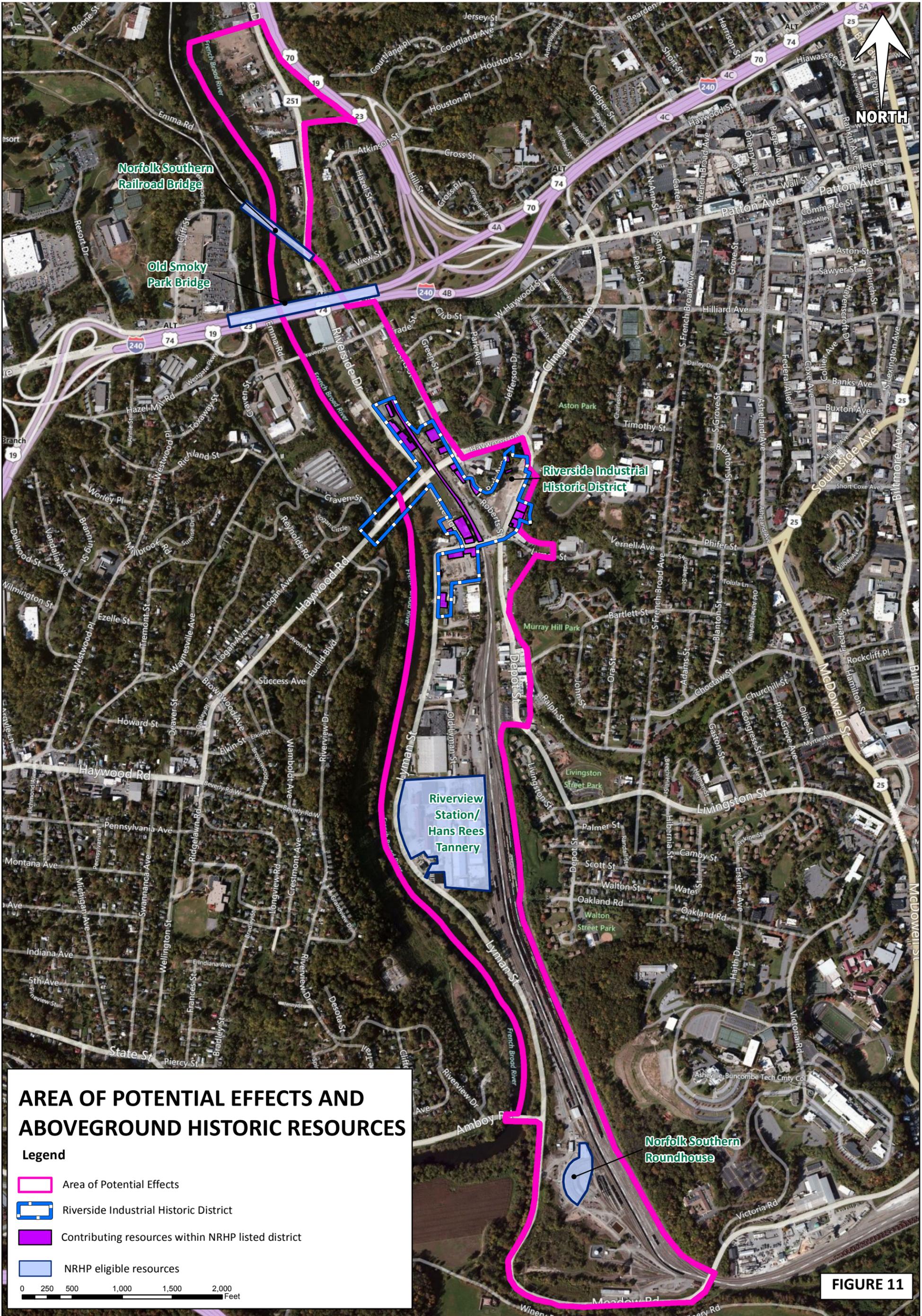


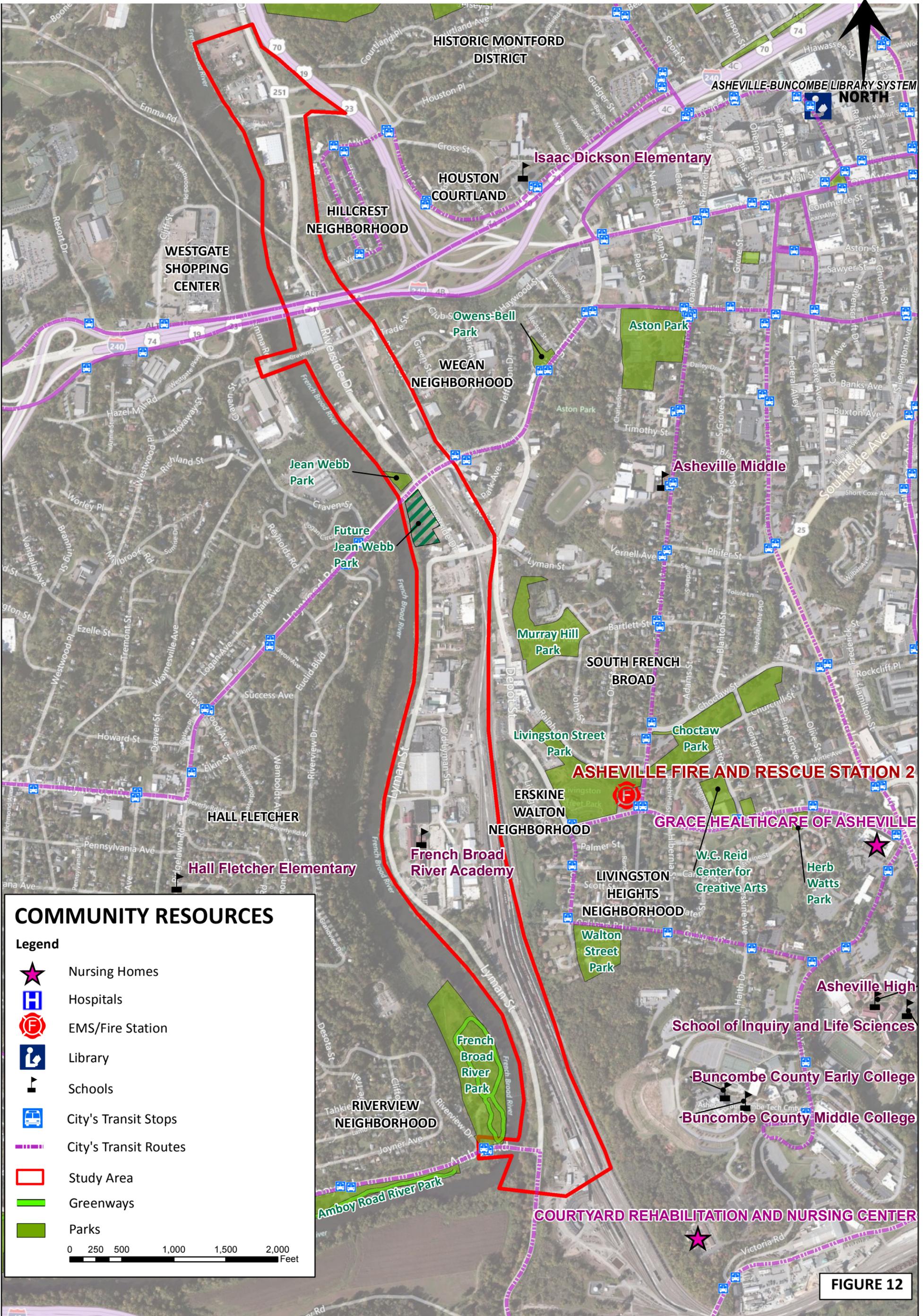
RIVER ARTS DISTRICT TRANSPORTATION PROJECT

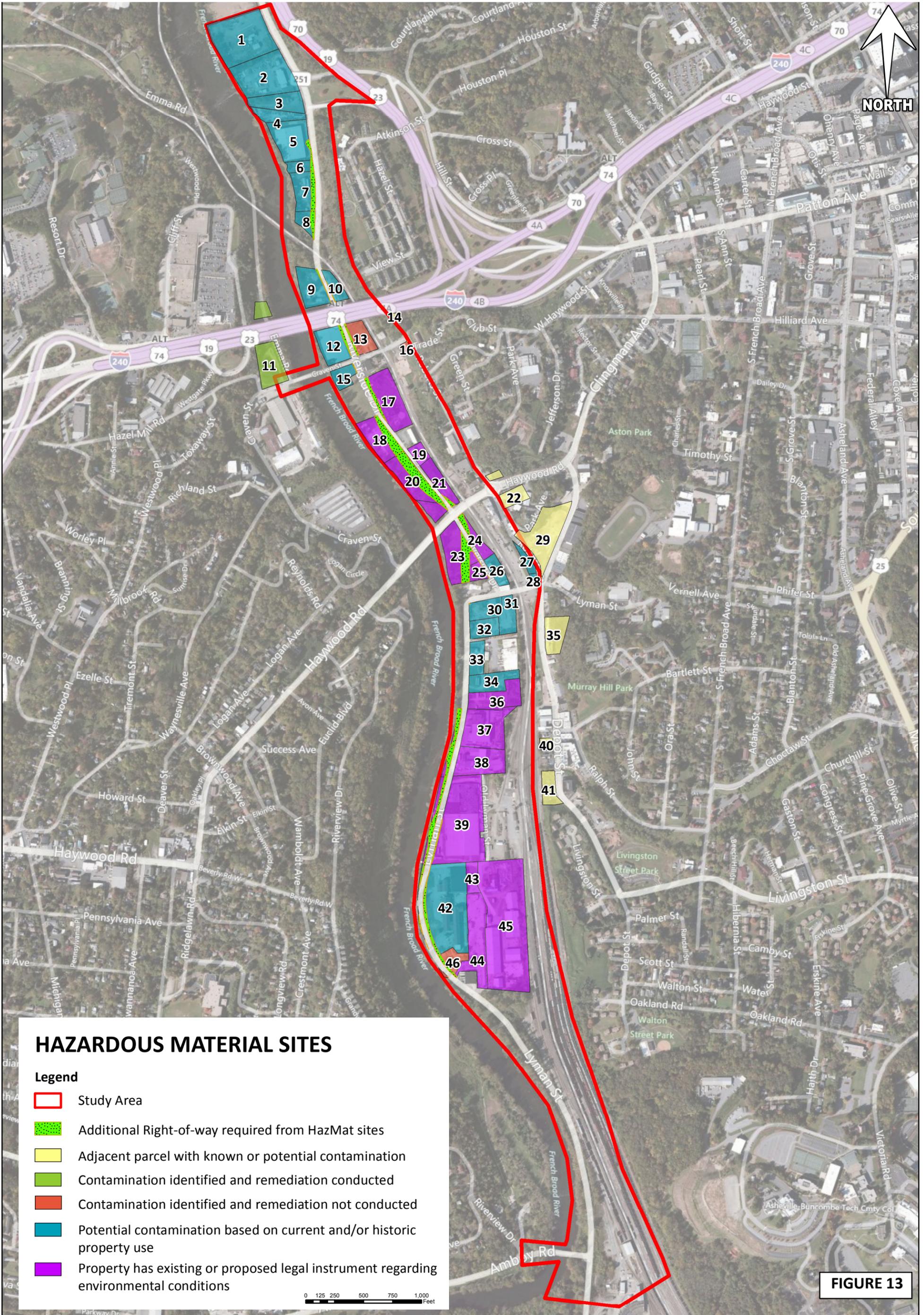
A Section of the Wilma Dykeman Riverway











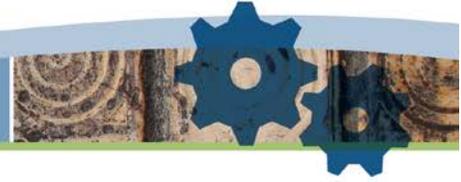
HAZARDOUS MATERIAL SITES

Legend

- Study Area
- Additional Right-of-way required from HazMat sites
- Adjacent parcel with known or potential contamination
- Contamination identified and remediation conducted
- Contamination identified and remediation not conducted
- Potential contamination based on current and/or historic property use
- Property has existing or proposed legal instrument regarding environmental conditions

0 125 250 500 750 1,000 Feet

FIGURE 13



APPENDIX B

Section 4(f) Discussion



Overview of Section 4(f) Regulations

Under Section 4(f) of the Department of Transportation Act, FHWA “may not approve the used of land from a significant publicly-owned park, recreation area, or wildlife and waterfowl refuge, or any significant historic site unless a determination is made that: (i) There is no feasible and prudent alternative to the use of land from the property; and (ii) The action includes all possible planning to minimize harm to the property resulting from such use.”

Under SAFETEA-LU, a *de minimis* programmatic agreement was established for minor uses. An impact to a park, recreation area, or wildlife and waterfowl refuge may be determined to be *de minimis* if:

1. The transportation use of the Section 4(f) resource (including any enhancements or mitigation) does not adversely affect the activities, features, or attributes that qualify the resource for protection under Section 4(f);
2. The official with jurisdiction over the property are informed of FHWA’s intent to make the *de minimis* impact finding based on their written concurrence that the project will not adversely affect the activities, features, and attributes that qualify the property for protection under Section 4(f); and
3. The public has been afforded an opportunity to review and comment on the effects of the project on the Section 4(f) resource.

Implementation of the River Arts District Transportation Improvement Project will convert portions of two Section 4(f) properties to transportation uses: the Riverside Industrial Historic District and the former Hans Rees Tannery.

Riverside Industrial Historic District

The Riverside Industrial Historic District was listed on the National Register of Historic Places in 2004. The district covers 25 acres, roughly bounded by Clingman Avenue, Lyman Street, Roberts Street and Riverside Drive. At the time of its listing, the district contained 28 contributing elements. Since its listing, three metal warehouses in the Dave Steele complex have been demolished.

As part of the River Arts District transportation improvement project, the district boundary was recommended for expansion to the south. The expanded boundary covers an additional 1.8 acres corresponding to the parcel containing two brick buildings associated with the Texas Oil Company. Both buildings are recommended as contributing elements within the expanded district.

The Preferred Alternative (Yellow/Green) for the project requires 0.37 acres of additional right-of-way within the district. These areas represent small areas adjacent to existing roadways and would not lead to direct effects on any structures within the district. The Yellow Alternative (not preferred) would require an additional 0.47 acres of right-of-way within the district; again, these areas are adjacent to roadway corridors and would not directly impact any structures within the district.

To minimize these impacts, the proposed alignment was shifted to the west to minimize the footprint within the district. In addition, the cross-section was minimized within the footprint of the district, eliminating on-street parking and the medians.

In a meeting with the SHPO on August 21, 2012, the project team discussed the project’s effects on the district. FHWA, NCDOT, the SHPO, and the City agreed that the Preferred Alternative does not result in



an adverse effect upon the historic district. Likewise, all parties agreed that a de minimis finding is appropriate as the Preferred Alternative will not adversely affect the activities, features, or attributes of the Riverside Industrial Historic District. Public comments will be solicited at the public hearing following the publication of this EA.

Former Hans Rees Tannery

The former Hans Rees Tannery is located at 191 Lyman Street. In 1898, two of Hans Rees' sons started the tannery in Asheville to be close to the source of the tanning agent, a supply of clean water for the process, access to the railroad for receiving the hides and shipment of the finished product, and the supply of a labor force. The tannery contained approximately 30 buildings and employed approximately 250 workers. A flood in 1916 and fire in 1917 demolished many of the early tannery structures. In the early years of the tannery's operation, the company produced leather belts for transmissions in heavy machinery; Hans Rees invented this process. Later, the facility specialized in leather shoes and saddles as other materials replaced the leather machine belts. The plant was declining by the late 1940s.

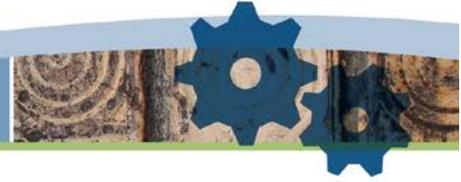
Today, the site includes 14 individual structures, several of which feature the tannery's characteristic stepped parapets at the tops of endwalls and internal firewalls. A three building brick complex on the west side of the property has been repurposed to serve as a series of artist studios and classroom spaces. A number of older structures on the site have deteriorated and been demolished. Several warehouses dated to the 1960s and later stand at the eastern edge of the property.

The site was determined to be eligible for listing on the National Register of Historic Places under Criteria A and C. The Hans Rees Tannery significantly contributed to the growth of the tannery industry in Western North Carolina. It is associated with events that have contributed significantly to the broad patterns of local and regional history, specifically the development of the tanning industry. The complex is also recommended eligible under Criterion C for its design and construction as an excellent example of an early twentieth century industrial complex.

The Preferred Alternative (Yellow/Green) for the project requires 0.7 acres of additional right-of-way within the tannery property. This represents a strip taking of additional right-of-way along Lyman Street, running along the western boundary of the property. The right-of-way would not lead to direct effects on any structures within the district and will not impact the parking areas serving the repurposed commercial studio spaces. The Purple Alternative (not preferred) would require an additional 2.4 acres of right-of-way and bisects the tannery property; it would result in an adverse effect on the resource.

To minimize these impacts, the proposed alignment for the Preferred Alternative was shifted to the west to minimize the footprint within the tannery. In addition, the cross-section was minimized within the footprint of the tannery, reducing on-street parking and the medians.

In a meeting with the SHPO on August 21, 2012, the project team discussed the project's effects on the district. FHWA, NCDOT, the SHPO, and the City agreed that the Preferred Alternative does not result in an adverse effect upon the former Hans Rees Tannery. Likewise, all parties agreed that a de minimis finding is appropriate as the Preferred Alternative will not adversely affect the activities, features, or attributes of the resource. Public comments will be solicited at the public hearing following the publication of this EA.



APPENDIX C

Coordination Letters from

Agencies



North Carolina Department of Environment and Natural Resources
Office of Conservation, Planning, and Community Affairs

Beverly Eaves Perdue, Governor

Linda Pearsall, Director

Dee Freeman, Secretary

September 20, 2010

RECEIVED
ERD 9/24/10

James D. Cutler, PWS
Senior Scientist
MACTEC Engineering and Consulting, Inc.
1308 Patton Avenue
Asheville, NC 28806

Re: MACTEC Project 6685-10-1888 (River arts District Section of the Wilma Dykeman Parkway)

Dear Mr. Cutler,

The North Carolina Natural Heritage Program received your request to review the above-mentioned project and has conducted a database search of rare plants, animals, and natural communities within the project area. No extant records for rare, threatened, or endangered species are known from the proposed project area. However, historical records for the following aquatic species are documented from the French Broad River adjacent to the site: Appalachian elktoe (*Alasmidonta raveneliana* – Federally Endangered), tan riffleshell (*Epioblasma florentina walteri* – Federally Endangered), and paddlefish (*Polypodon spathula* – Federal Species of Concern).

NC Natural Heritage Program recommends that the applicant also consult with US Fish and Wildlife Service as well as the NC Wildlife Resources Commission in order to verify that no more recent record for these or other listed species is known from the project area.

Sincerely,

Edward Schwartzman
Inventory Biologist
NC Natural Heritage Program

CC: Harry Legrand (NC Natural Heritage Program)



RECEIVED
10/8/10 APD

☒ North Carolina Wildlife Resources Commission ☒

Gordon Myers, Executive Director

October 5, 2010

Mr. Richard G. Harmon
MACTEC Engineering and Consulting, Inc.
1308 Patton Avenue
Asheville, North Carolina 28806

SUBJECT: River Arts Section of the Wilma Dykeman Riverway Project
Buncombe County

Dear Mr. Wright:

Biologists with the North Carolina Wildlife Resources Commission (Commission) reviewed your September 24, 2010 letter about the River Arts Section of the Wilma Dykeman Riverway project. You requested that we comment on the potential beneficial or adverse impacts of the project. Comments from the Commission are provided under provisions of the Fish and Wildlife Coordination Act (48 Stat. 401, as amended; 16 U.S.C. 661 et seq.) and the North Carolina Environmental Policy Act (G.S. 113A-1 et seq., as amended; 1 NCAC-25).

There is insufficient information in your letter to assess the potential beneficial or adverse impacts of the project. For example, impacts to streams and wetlands are not quantified. We do not have records for rare species on the property (whether it has been surveyed is unknown), but there are several known to occur in and along the French Broad River near the property. These include, but are not limited to, the southern blotched chub (*Erimystax insignis eristigma*, US and NC SC), mole salamander (*Ambystoma talpodeum*, NC SC), paddlefish (*Polyodon spathula*, NC E and US FSC). The river also supports smallmouth bass and muskellunge sport fisheries.

The Commission appreciates the opportunity to provide comments on this project. If you need to discuss these comments please call me at (828) 452-0422 extension 24.

Sincerely,

Dave McHenry, Mountain Region Coordinator, Habitat Conservation Program

Mailing Address: Division of Inland Fisheries • 1721 Mail Service Center • Raleigh, NC 27699-1721
Telephone: (919) 707-0220 • **Fax:** (919) 707-0028



United States Department of the Interior

FISH AND WILDLIFE SERVICE

Asheville Field Office
160 Zillicoa Street
Asheville, North Carolina 28801

October 19, 2010



Mr. Robert L. Sain, CE, CFM
Project Scientist
MACTEC Engineering and Consulting, Inc.
1308 Patton Avenue
Asheville, North Carolina 28806

Dear Mr. Sain:

Subject: Endangered and Threatened Species Assessment, River Arts District, Wilma Dykeman Riverway Project, Asheville, Buncombe County, North Carolina

We received your August 31, 2010, letter in which you requested our comments on the Wilma Dykeman Riverway project. We have reviewed the information you presented and are providing the following comments in accordance with the provisions of the Fish and Wildlife Coordination Act, as amended (16 U.S.C. 661-667e), and section 7 of the Endangered Species Act of 1973, as amended (16 U.S.C. 1531-1543) (Act).

According to the information presented, the proposed project consists of about 2.2 miles of roadway and infrastructure improvements along Riverside Drive and Lyman Street in Asheville's River Arts District. Roadway improvements may include roadway relocation and widening, greenway facilities, bridge reconstruction, railroad crossing upgrades, sidewalks, streetscapes, and transit amenities. The project area is predominantly urbanized with residential, commercial, and industrial development and contains some areas of fragmented and degraded forest habitat.

Endangered Species. According to our records, no listed species or their habitats occur on the site. We do not believe any endangered or threatened species or their habitats will be affected by the proposed project; therefore, the requirements under section 7 of the Act are fulfilled. However, obligations under section 7 of the Act must be reconsidered if: (1) new information reveals impacts of this identified action that may affect listed species or critical habitat in a manner not previously considered, (2) this action is subsequently modified in a manner that was not considered in this review, or (3) a new species is listed or critical habitat is determined that may be affected by the identified action.

Fish and Wildlife Resources - We are concerned about the direct, indirect, and cumulative impacts of this project to aquatic and terrestrial resources on the subject property. It is important that you minimize or avoid impacts to the resources located on the property, and any unavoidable impacts will require mitigation. We offer the following recommendations to help address the impacts associated with this project and to help minimize impacts to fish and wildlife resources:

1. Preserve and/or restore forested riparian buffers. Given the close proximity of this project to aquatic resources and the increased amount of impervious surface area that will occur as a result of the development, we are concerned about the loss and lack of riparian buffers. Forested riparian buffers, a minimum of 100 feet wide along perennial streams and 50 feet wide along intermittent streams, should be created and/or maintained along all aquatic areas. Riparian buffers provide travel corridors and habitat for wildlife displaced by development. In addition, riparian buffers protect water quality by stabilizing stream banks, filtering storm-water runoff, and providing habitat for aquatic and fishery resources.
2. Integrate low-impact-development techniques into project plans and minimize the amount of impervious surface area to the greatest extent practicable. In addition to the increased storm-water flows caused by the lack of or loss of riparian buffers and any floodplain development, increased development outside the floodplain will also contribute to the quantity and quality of storm water entering project area waterways. Recent studies¹ have shown that areas of 10- to 20-percent impervious surface (such as roofs, roads, and parking lots) double the amount of storm-water runoff (compared to natural cover) and decrease deep infiltration (groundwater recharge) by 16 percent. At 35- to 50-percent impervious surface, runoff triples, and deep infiltration is decreased by 40 percent. Above 75-percent impervious surface, runoff is 5.5 times higher than with natural cover, and deep infiltration is decreased by 80 percent.

These impervious surfaces collect pathogens, metals, sediment, and chemical pollutants and quickly transmit them (via storm-water runoff) to receiving waters. According to the Environmental Protection Agency, this nonpoint-source pollution is one of the major threats to water quality in the United States, posing one of the greatest threats to aquatic life, and is also linked to chronic and acute illnesses in human populations from exposure through drinking water and contact recreation. The adequate treatment of storm water in development areas is essential for the protection of water quality and aquatic habitat in developing landscapes. Increased storm-water runoff also directly damages aquatic and riparian habitat, causing stream-bank and stream-channel scouring. In addition, impervious surfaces reduce groundwater recharge, resulting in even lower than expected stream flows during drought periods, which can induce potentially catastrophic effects for fish, mussels, and other aquatic life. Accordingly, we recommend that all new developments, regardless of the percentage of impervious

¹Federal Interagency Stream Restoration Working Group (15 federal agencies of the United States Government). October 1998. Stream Corridor Restoration: Principles, Processes, and Practices. GPO Item No. 0120-A; SuDocs No. A 57.6/2:EN 3/PT.653. ISBN-0-934213-59-3.

surface area they will create, implement storm-water-retention and -treatment measures designed to replicate and maintain the hydrograph at the preconstruction condition in order to avoid any additional impacts to habitat quality within the watershed.

We recommend the use of low-impact-development techniques,² such as reduced road widths, grassed swales in place of curb and gutter, rain gardens, and wetland retention areas, for retaining and treating storm-water runoff rather than the more traditional measures, such as large retention ponds, etc. These designs often cost less to install and significantly reduce environmental impacts from residential development. Where detention ponds are used, storm-water outlets should drain through a vegetated area prior to reaching any natural stream or wetland area. Detention structures should be designed to allow for the slow discharge of storm water, attenuating the potential adverse effects of storm-water surges; thermal spikes; and sediment, nutrient, and chemical discharges. Also, because the purpose of storm-water-control measures is to protect streams and wetlands, no storm-water-control measures or best management practices should be installed within any stream (perennial or intermittent) or wetland. We also recommend that consideration be given to the use of pervious materials (i.e., pervious concrete, interlocking/open paving blocks, etc.) for the construction of roads, driveways, sidewalks, etc. Pervious surfaces minimize changes to the hydrology of the watershed and can be used to facilitate groundwater recharge. Pervious materials are also less likely to absorb and store heat and allow the cooler soil below to cool the pavement. Additionally, pervious concrete requires less maintenance and is less susceptible to freeze/thaw cracking due to large voids within the concrete.

3. Install and maintain stringent measures to control erosion and sediment in order to prevent unnecessary impacts to aquatic resources within and downstream of the project site. Disturbed areas should be reseeded with seed mixtures that are beneficial to wildlife. Fescue-based mixtures should be avoided. Native annual small grains appropriate for the season are preferred and recommended.
4. Use bridges for all permanent roadway crossings of streams and associated wetlands. All stream crossings should be made perpendicular to the stream. We recommend bridges that span the entire floodplain because it is important for streams to have access/connectivity to the floodplain. Bridges that span the stream and floodplain are the best option because they minimize impacts to aquatic resources, allow for the movement of aquatic organisms, and eliminate the need to place fill in streams and floodplains. Bridges should be designed and constructed so that no piers or bents are placed in the stream, and approaches and abutments should not constrict the stream channel. Bridges should also be designed to allow for safe terrestrial wildlife passage. To provide for terrestrial wildlife passage, the new bridge design should span beyond the waterway so that

²We recommend visiting the Environmental Protection Agency's Web site (<http://www.epa.gov/owow/nps/lid/>) for additional information and fact sheets regarding the implementation of low-impact-development techniques.

unsubmerged land is also bridged. If bank stabilization is necessary, we recommend that the use of riprap be minimized and that a riprap-free buffer zone be maintained under the bridge to allow for wildlife movement. Longer bridge spans also cost far less than a separate wildlife crossing under an existing roadway. Also, floodplain culverts must be installed if fill is placed in the floodplain for bridge construction.

We appreciate the opportunity to provide these comments. If we can be of assistance or if you have any questions, please do not hesitate to contact Mr. Bryan Tompkins of our staff at 828/258-3939, Ext. 240. In any future correspondence concerning this project, please reference our Log Number 4-2-10-303.

Sincerely,

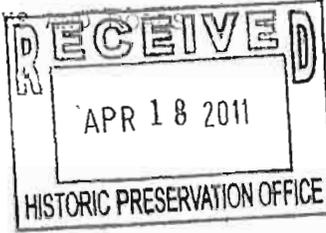
A handwritten signature in black ink, appearing to read "Brian P. Cole". The signature is fluid and cursive, with a long horizontal stroke at the end.

Brian P. Cole
Field Supervisor

NORTH CAROLINA STATE CLEARINGHOUSE
DEPARTMENT OF ADMINISTRATION
INTERGOVERNMENTAL REVIEW

COUNTY: BUNCOMBE

F02: HIGHWAYS



STATE NUMBER: 11-E-0000-0252
DATE RECEIVED: 04/15/2011
AGENCY RESPONSE: 05/25/2011
REVIEW CLOSED: 05/30/2011

MS RENEE GLEDHILL-EARLEY
CLEARINGHOUSE COORDINATOR
DEPT OF CULTURAL RESOURCES
STATE HISTORIC PRESERVATION OFFICE
MSC 4617 - ARCHIVES BUILDING
RALEIGH NC

REVIEW DISTRIBUTION

CC&PS - DIV OF EMERGENCY MANAGEMENT
DENR LEGISLATIVE AFFAIRS
DEPT OF AGRICULTURE
DEPT OF CULTURAL RESOURCES
DEPT OF TRANSPORTATION
LAND OF SKY REGIONAL COUNCIL

PROJECT INFORMATION

APPLICANT: City of Asheville
TYPE: National Environmental Policy Act
Scoping

DESC: Proposal of the U-5019, Wilma Dykeman Riverway project development will focus on a 2.2 mile section of roadway including the corridor of Lyman Street and a portion of Riverside Drive from Amboy Road (SR 3556) to Hill Street (SR 1231) - roadway relocations and widening, on-street parking, intersection and signal upgrades, road crossing improvements, bridge reconstruction are part of the roadway and infrastructure improvements

The attached project has been submitted to the N. C. State Clearinghouse for intergovernmental review. Please review and submit your response by the above indicated date to 1301 Mail Service Center, Raleigh NC 27699-1301.

If additional review time is needed, please contact this office at (919)807-2425.

AS A RESULT OF THIS REVIEW THE FOLLOWING IS SUBMITTED: NO COMMENT COMMENTS ATTACHED

SIGNED BY:

Renee Gledhill-Earley

DATE:

5.19.11

*CH 11-0645
Draft letter -
A- LGH/BJS 4-26-11
Due 4/27/11*

Due 5/4/11 See letter JJK 5/6/11



APR 19 2011

NORTH CAROLINA STATE CLEARINGHOUSE
DEPARTMENT OF ADMINISTRATION
INTERGOVERNMENTAL REVIEW

COUNTY: BUNCOMBE

F02: HIGHWAYS AND ROADS

STATE NUMBER: 11-E-0000-0252

DATE RECEIVED: 04/15/2011

AGENCY RESPONSE: 05/25/2011

REVIEW CLOSED: 05/30/2011

CLEARINGHOUSE COORDINATOR
CC&PS - DIV OF EMERGENCY MANAGEMENT
FLOODPLAIN MANAGEMENT PROGRAM
MSC # 4719
RALEIGH NC

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DEPT OF CULTURAL RESOURCES
DEPT OF TRANSPORTATION
LAND OF SKY REGIONAL COUNCIL

PROJECT INFORMATION

APPLICANT: City of Asheville
TYPE: National Environmental Policy Act
Scoping



DESC: Proposal of the U-5019, Wilma Dykeman Riverway project development will focus on a 2.2 mile section of roadway including the corridor of Lyman Street and a portion of Riverside Drive from Amboy Road (SR 3556) to Hill Street (SR 1231) - roadway relocations and widening, on-street parking, intersection and signal upgrades, road crossing improvements, bridge reconstruction are part of the roadway and infrastructure improvements

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If additional review time is needed, please contact this office at (919)807-2425.

AS A RESULT OF THIS REVIEW THE FOLLOWING IS SUBMITTED: NO COMMENT COMMENTS ATTACHED

SIGNED BY: _____
[Handwritten Signature]

DATE: 5/26/11

[Faint handwritten notes]

Memorandum

To: Ron Townley
From: Kate O'Hara, Senior Planner/BF Program Manager
Date: 4/25/2011
Re: COA State Environmental Assessment/FONSI- Wilma
Dykeman Riverway Project

With regard to parcels in the study area where potential hazardous materials may be present, consultation with the NCDENR Brownfields Program, NCDENR UST Program and the Regional Brownfields Initiative is encouraged.

While several sites within the study area are listed with various regulatory agencies, consultation on individual sites may provide more specific information with regard to environmental assessments performed, LURS pending and more.

Additionally , an opportunity for further environmental assessments may exist if future funding is available to the COA under the Brownfields Program.

memo

Land-of-Sky Regional Council and French Broad MPO

To: Ron
From: Lyuba
Date: 4/25/2011
Re: Comments on Wilma Dykeman Riverway Project

Ron, in reviewing the scope of the Environmental Assessment/FONSI Study for the Wilma Dykeman Riverway Project, I have several general comments. Specifically:

- Overall, this project will benefit the redevelopment of the River Arts District, and will help tie together multiple residential and commercial neighborhoods, specifically West Asheville, River Arts District, Montford and "Chicken Hill" neighborhoods; this project should be beneficial from the livability, alternative transportation, congestion mitigation, public health, tourism and economic development perspectives.
- Safe transitions and crossings from the future greenway to the neighborhood sidewalks and roadways are critical. For example, a safe and pedestrian and bicycle-friendly connection to Hill Street roadway and sidewalks would allow a walkable and bikeable connection to Montford; curb cuts in the greenway at appropriate locations could ensure that the bicyclists can transition from the city streets to the greenway, and vice versa.
- There is a "pinch point" along the route at approximately 314 Riverside Drive, where the Riverside Drive roadway currently crosses under a railroad bridge with only 12 feet of height clearance. This bridge underpass is very narrow and too low for most tractor trailers that might be making deliveries in the area. Particular attention should be paid to this spot. If the railroad bridge has to be replaced to allow for greenway crossing, an increase in height should be considered to allow better local access for delivery trucks.
- At a location near 352 Riverside Drive, railroad tracks cross Riverside at a diagonal. If the greenway parallels Riverside Drive in this area, special care must be taken to provide a safe crossing across the train tracks, especially critical for bicyclists. A wider path section might be needed to correct for the angle of crossing. Oregon DOT recommends that "if the skew angle is less than 45°, special attention should be given to the bikeway alignment to improve the angle of approach, preferably to 60° or greater, so cyclists can avoid catching their wheels in the flange and losing their balance." Maryland DOT *Bicycle and Pedestrian Design Guidelines* (Section 4.7, <http://www.sha.maryland.gov/QOTS/Chapter%204%20-%20Other%20On-Road.pdf>) provides an example of jug handle alignment solution that puts cyclists at a 90° crossing angle to the train tracks. See image on the next page.

Division of Water Quality
Asheville Regional Office
Surface Water Protection Section
April 29, 2011

Memorandum

To: Melba McGee, Environmental Coordinator

From: Susan A. Wilson, Environmental Engineer 

Subject: Wilma Dykeman Riverway Project
Environmental Review Record Comments
Buncombe County

The City of Asheville proposes to construct a multi-access parkway along the French Broad River (as well as the Swannanoa River). This proposal is for a 2.2 mile section of the 17 miles Wilma Dykeman Riverway corridor.

The following concerns may, or will likely, need to be addressed:

1. NPDES Construction Stormwater Permit NCG010000 – This permit is issued concurrently with an approved Erosion and Sediment Control plan to control stormwater discharges from construction activities. Likely the proposed project will disturb greater than one acre, so an NCG010000 permit is required.
2. 401 Water Quality Certification – If a 404 permit is required by the Army Corp of Engineers, a 401 Water Quality Certification is necessary. Depending on the amount of impact to a stream and/or wetland – written concurrence from this office may be required. The DWQ is unsure of the total jurisdictional waters or wetlands for the area – this should be confirmed prior to impacts to the site.
3. Asheville and Buncombe County Phase II stormwater requirements - the City (or its consultant), should ensure compliance with Phase II stormwater rules or additional stormwater requirements. The percentage built upon area of the project may require post construction stormwater controls. Impervious surface areas should be 30 feet from surface waters.

Thank you for the opportunity to comment. Please contact me or Chuck Cranford at (828) 296-4500 (susan.a.wilson@ncdner.gov; chuck.cranford@ncdenr.gov) should you have any questions regarding this memo.

cc: ARO files
Buncombe County Planning

S:\SWP\Buncombe\Z-Loose Docs\A95 Scoping docs\Dykeman Riverway Project Asheville River Dist.4 2011.doc

INTERGOVERNMENTAL REVIEW - PROJECT COMMENTS

Project Number: 11-0252 Due Date: 5-23-11

After review of this project it has been determined that the ENR permit(s) and/or approvals indicated may need to be obtained in order for this project to comply with North Carolina Law. Questions regarding these permits should be addressed to the Regional Office indicated on the reverse of the form. All applications, information and guidelines relative to these plans and permits are available from the same Regional Office.

	PERMITS	SPECIAL APPLICATION PROCEDURES or REQUIREMENTS	Normal Process Time (statutory time limit)
<input type="checkbox"/>	Permit to construct & operate wastewater treatment facilities, sewer system extensions & sewer systems not discharging into state surface waters.	Application 90 days before begin construction or award of construction contracts. On-site inspection. Post-application technical conference usual.	30 days (90 days)
<input type="checkbox"/>	NPDES - permit to discharge into surface water and/or permit to operate and construct wastewater facilities discharging into state surface waters	Application 180 days before begin activity. On-site inspection. Pre-application conference usual. Additionally, obtain permit to construct wastewater treatment facility granted after NPDES. Reply time, 30 days after receipt of plans or issue of NPDES permit-whichever is later.	90-120 days (N/A)
<input type="checkbox"/>	Water Use Permit	Pre-application technical conference usually necessary	30 days (N/A)
<input type="checkbox"/>	Well Construction Permit	Complete application must be received and permit issued prior to the installation of a well.	7 days (15 days)
<input type="checkbox"/>	Dredge and Fill Permit	Application copy must be served on each adjacent riparian property owner. On-site inspection. Pre-application conference usual. Filling may require Easement to Fill from N.C. Department of Administration and Federal Dredge and Fill Permit.	55 days (90 days)
<input type="checkbox"/>	Permit to construct & operate Air Pollution Abatement facilities and/or Emission Sources as per 15 A NCAC (2Q.0100 thru 2Q.0300)	Application must be submitted and permit received prior to construction and operation of the source. If a permit is required in an area without local zoning, then there are additional requirements and timelines (2Q.0113).	90 days
<input type="checkbox"/>	Permit to construct & operate Transportation Facility as per 15 A NCAC (2D.0800, 2Q.0601)	Application must be submitted at least 90 days prior to construction or modification of the source.	90 days
<input checked="" type="checkbox"/>	Any open burning associated with subject proposal must be in compliance with 15 A NCAC 2D.1900	N/A	60 days (90 days)
<input type="checkbox"/>	Demolition or renovations of structures containing asbestos material must be in compliance with 15 A NCAC 20.1110 (a) (1) which requires notification and removal prior to demolition. Contact Asbestos Control Group 919-707-5950.		
<input type="checkbox"/>	Complex Source Permit required under 15 A NCAC 2D.0800		
<input type="checkbox"/>	The Sedimentation Pollution Control Act of 1973 must be properly addressed for any land disturbing activity. An erosion & sedimentation control plan will be required if one or more acres to be disturbed. Plan filed with proper Regional Office (Land Quality Section) At least 30 days before beginning activity. A fee of \$65 for the first acre or any part of an acre. An express review option is available with additional fees.		20 days (30 days)
<input checked="" type="checkbox"/>	Sedimentation and erosion control must be addressed in accordance with NCDOT's approved program. Particular attention should be given to design and installation of appropriate perimeter sediment trapping devices as well as stable stormwater conveyances and outlets.		(30 days)
<input type="checkbox"/>	Mining Permit	On-site inspection usual. Surety bond filed with ENR Bond amount varies with type mine and number of acres of affected land. Any acre mined greater than one acre must be permitted. The appropriate bond must be received before the permit can be issued.	30 days (60 days)
<input type="checkbox"/>	North Carolina Burning permit	On-site inspection by N.C. Division Forest Resources if permit exceeds 4 days	1 day (N/A)
<input type="checkbox"/>	Special Ground Clearance Burning Permit - 22 counties in coastal N.C. with organic soils	On-site inspection by N.C. Division Forest Resources required "If more than five acres of ground clearing activities are involved. Inspections should be requested at least ten days before actual burn is planned."	1 day (N/A)
<input type="checkbox"/>	Oil Refining Facilities	N/A	90-120 days (N/A)
<input type="checkbox"/>	Dam Safety Permit	If permit required, application 60 days before begin construction. Applicant must hire N.C. qualified engineer to: prepare plans, inspect construction, certify construction is according to ENR approved plans. May also require permit under mosquito control program. And a 404 permit from Corps of Engineers. An inspection of site is necessary to verify Hazard Classification. A minimum fee of \$200.00 must accompany the application. An additional processing fee based on a percentage of the total project cost will be required upon completion.	30 days (60 days)

PERMITS		SPECIAL APPLICATION PROCEDURES or REQUIREMENTS	Normal Process Time (statutory time limit)
<input type="checkbox"/>	Permit to drill exploratory oil or gas well	File surety bond of \$5,000 with ENR running to State of NC conditional that any well opened by drill operator shall, upon abandonment, be plugged according to ENR rules and regulations.	10 days N/A
<input type="checkbox"/>	Geophysical Exploration Permit	Application filed with ENR at least 10 days prior to issue of permit. Application by letter. No standard application form.	10 days N/A
<input type="checkbox"/>	State Lakes Construction Permit	Application fees based on structure size is charged. Must include descriptions & drawings of structure & proof of ownership of riparian property.	15-20 days N/A
<input type="checkbox"/>	401 Water Quality Certification	N/A	60 days (130 days)
<input type="checkbox"/>	CAMA Permit for MAJOR development	\$250.00 fee must accompany application	55 days (150 days)
<input type="checkbox"/>	CAMA Permit for MINOR development	\$50.00 fee must accompany application	22 days (25 days)
<input type="checkbox"/>	Several geodetic monuments are located in or near the project area. If any monument needs to be moved or destroyed, please notify: N.C. Geodetic Survey, Box 27687 Raleigh, NC 27611		
<input type="checkbox"/>	Abandonment of any wells, if required must be in accordance with Title 15A. Subchapter 2C.0100.		
<input type="checkbox"/>	Notification of the proper regional office is requested if "orphan" underground storage tanks (USTS) are discovered during any excavation operation.		
<input type="checkbox"/>	Compliance with 15A NCAC 2H 1000 (Coastal Stormwater Rules) is required.		45 days (N/A)
<input type="checkbox"/>	Tar Pamlico or Neuse Riparian Buffer Rules required.		
* Other comments (attach additional pages as necessary, being certain to cite comment authority)			

REGIONAL OFFICES

Questions regarding these permits should be addressed to the Regional Office marked below.

Asheville Regional Office
2090 US Highway 70
Swannanoa, NC 28778
(828) 296-4500

Mooresville Regional Office
610 East Center Avenue, Suite 301
Mooresville, NC 28115
(704) 663-1699

Wilmington Regional Office
127 Cardinal Drive Extension
Wilmington, NC 28405
(910) 796-7215

Fayetteville Regional Office
225 North Green Street, Suite 714
Fayetteville, NC 28301-5043
(910) 433-3300

Raleigh Regional Office
3800 Barrett Drive, Suite 101
Raleigh, NC 27609
(919) 791-4200

Winston-Salem Regional Office
585 Woughtown Street
Winston-Salem, NC 27107
(336) 771-5000

Washington Regional Office
943 Washington Square Mall
Washington, NC 27889
(252) 946-6481



North Carolina Department of Cultural Resources
State Historic Preservation Office

Claudia Brown, Acting Administrator

Beverly Eaves Perdue, Governor
Linda A. Carlisle, Secretary
Jeffrey J. Crow, Deputy Secretary

Office of Archives and History
Division of Historical Resources
David Brook, Director

May 11, 2011

Dan Baechtold
City of Asheville
PO Box 7148
Asheville, NC 28802

Re: Wilma Dykeman Riverway Project, U-5019, Buncombe County, CH 11-0645

Dear Mr. Baechtold:

We have received notification from the State Clearinghouse concerning the above project.

The project description references an archaeological investigation undertaken to initially assess the impact to archaeological resources that may be in the project area. Please send two copies of the archaeological survey report, as well as one copy of the appropriate site forms, to us for review.

Please note, several historic properties are located within the Area of Potential Effect for this project, including:

- ◆ Biltmore Estate (BN 1835), a National Historic Landmark;
- ◆ Riverside Industrial Historic District (BN 1827), listed in the National Register;
- ◆ Hans Rees Sons Tannery (BN 0414), on the State Study List; and,
- ◆ Parsonage (BN 1401), a surveyed site.

We are unable to comment on the potential effect of this project on historic properties until we receive further information, including a detailed description and photographs of the architectural and historic resources within the Area of Potential Effect and maps showing the proposed right-of-way, roadway configuration, and landscaping.

The above comments are made pursuant to Section 106 of the National Historic Preservation Act and the Advisory Council on Historic Preservation's Regulations for Compliance with Section 106 codified at 36 CFR Part 800.

Thank you for your cooperation and consideration. If you have questions concerning the above comment, please contact Renee Gledhill-Earley, environmental review coordinator, at 919-807-6579. In all future communication concerning this project, please cite the above-referenced tracking number.

Sincerely,

A handwritten signature in blue ink that reads "Renee Gledhill-Earley". The signature is written in a cursive style.

for Claudia Brown

cc: State Clearinghouse
Stacy Merten, smerten@ashevillenc.gov



May 16, 2011

MEMORANDUM

FROM: Tracy Wahl (NCDENR)
TO: Ron Townley (LOSRC)
RE: COA State Environmental Assessment/FONSI – Wilma Dykeman Riverway Project

Ron,

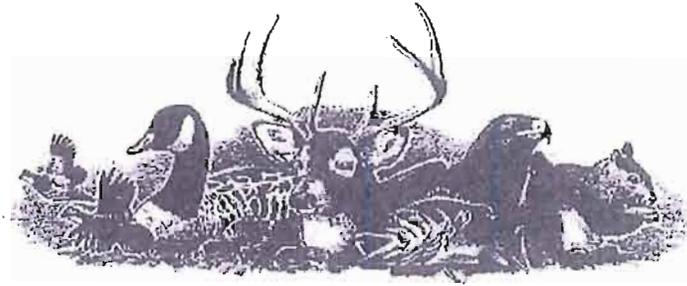
In regards to the Environmental Assessment of the Riverway Project, I have the following information related to properties located within this corridor:

1. Historic Cotton Mill, Brownfields Project Number 07015-03-11 located at 191 Riverside Drive has a recorded Brownfields Agreement at the Buncombe County Register Deeds, dated 7/6/2006. Soil and groundwater at the site are contaminated with VOCs and SVOCs. Uses are restricted to retail, commercial, and residential. With restrictions on surface and groundwater as well as excavation and/or disturbing soil.
2. B&H Sheet Metal, Brownfields Project Number 11034-07-11 located at 115-119 ½ Riverside Drive, is an active eligible site in the NCBP. Data has not been acquired on the site to date.
3. The Old Wood Company, Brownfields Project Number 14026-10-11 located at 99 Riverside Drive. Phase II assessment is complete with findings of SVOCs and elevated metals in soil and groundwater. Use of surface water or groundwater will be restricted along with excavation and/or disturbing of soil, unless prior DENR approval is obtained.
4. Ice Plant of Asheville, Brownfields Project Number 09033-05-11 located at 90 and 9999 Riverside Drive includes Parcels (9638.08-97-3660.00 and 9638.08-97-0681.00). The Brownfields Agreement has been recorded at the Buncombe County Register of Deeds, dated 12/17/2010. Soil at the site is contaminated with SVOCs, uses are restricted to: retail, hospitality, office, residential, and parks. Soil in the area of contamination requires removal, capping, or remediation approved by DENR. Surface water and groundwater are restricted.
5. Former Asheville MGP #1, Brownfields Project Number 13018-09-11 located at 14 Riverside Drive. Phase II assessment is complete, DENR has drafted a Brownfields Agreement. Based on soil and groundwater contamination with VOCs, SVOCs, and metals surface water and groundwater use will be restricted. Soil in the area of highest contamination requires removal, capping, or remediation approved by DENR.

Uses are restricted to office, retail, visitors/recreation center, greenways, and open space as defined by the Brownfields Agreement.

6. Asheville Waste Paper, Brownfields Project Number 09031-05-11 is located at 131 Lyman Street. Based on the assessments to date, soil and groundwater at the site are contaminated with VOCs, SVOCs, and metals. Restrictions will include surface water, groundwater, and soil in specific areas.
7. Day Warehouse, Brownfields Project Number 11007-07-11, is located at 336 Old Lyman Street. Initial assessment indicates that groundwater is contaminated with VOCs. Additional assessment has been requested for a property addition.

More detailed information is available for review by appointment.



North Carolina Wildlife Resources Commission

Gordon Myers, Executive Director

TO: Melba McGee, Environmental Coordinator
Office of Legislative and Intergovernmental Affairs, DENR

FROM: Marla Chambers, Western NCDOT Permit Coordinator *Marla Chambers*
Habitat Conservation Program, NCWRC

DATE: May 23, 2011

SUBJECT: Scoping review of a section of the Wilma Dykeman Riverway Project in Asheville, Buncombe County. TIP No. U-5019. OLIA Project No. 11-0252, due 5/23/2011.

The City of Asheville is preparing an Environmental Assessment/Finding of No Significant Impact (EA/FONSI) for the River Arts District Transportation Project, a section of the Wilma Dykeman Riverway (U-5019). The project study is funded by a United States Department of Transportation (USDOT) grant administered through the North Carolina Department of Transportation (NCDOT). The City is requesting comments from the North Carolina Wildlife Resources Commission (NCWRC) regarding impacts to fish and wildlife resources resulting from the subject project. Staff biologists have reviewed the information provided and have the following preliminary comments. These comments are provided in accordance with the provisions of the National Environmental Policy Act (42 U.S.C. 4332(2)(c)) and the Fish and Wildlife Coordination Act (48 Stat. 401, as amended; 16 U.S.C. 661-667d).

The project is a 2.2 mile section of the 17 mile Wilma Dykeman Riverway corridor along the French Broad and Swannanoa Rivers, proposed to be a "rebirth of the riverfronts in Asheville". The subject section runs along the east side of the French Broad River from the Hill Street (SR 1231)/I-240 interchange to the Lyman Street/Amboy Road (SR 3556) intersection. Project improvements under consideration include roadway relocations and widening, on-street parking, intersection and signal upgrades, railroad crossing improvements, bridge reconstruction, median treatments, sidewalks, streetscape elements, transit amenities, and greenway facilities.

We are concerned about the current and future water quality impacts to the French Broad River from this highly urbanized and industrial setting. We see this project as an opportunity to make significant improvements to stormwater treatment and other aspects of the area that tend to

degrade water quality and enjoyment of the river, the focus point of the project. Numerous studies have shown that when 10–15% of a watershed is converted to impervious surfaces, there is a serious decline in the health of receiving waters (Schueler 1994) and the quality of fish habitat and wetlands are negatively impacted (Booth 1991, Taylor 1993). We strongly encourage project planners to incorporate as many measures to treat stormwater runoff and to improve water quality as possible.

Reducing impervious coverage and limiting the amount of new impervious features will be important to this end. Rain gardens, green roofs, and pervious pavement for sidewalks and parking lots are just a few low impact development (LID) techniques that can be incorporated, often as attractive landscape features. Information on Low Impact Development practices and measures can be found at www.lowimpactdevelopment.org, <http://www.epa.gov/owow/nps/lid/lidnatl.pdf> and <http://www.stormwatercenter.net/>. We also encourage measures to mitigate secondary and cumulative impacts, which can be found in the Guidance Memorandum to Address and Mitigate Secondary and Cumulative Impacts to Aquatic and Terrestrial Wildlife Resources and Water Quality (NCWRC 2002).

In addition, to help facilitate document preparation and the review process, our general information needs are outlined below:

1. Description of fishery and wildlife resources within the project area, including a listing of federally or state designated threatened, endangered, or special concern species. Potential borrow areas to be used for project construction should be included in the inventories. A listing of designated plant species can be developed through consultation with the following programs:

The Natural Heritage Program
<http://www.ncnhp.org>
1601 Mail Service Center
Raleigh, N. C. 27699-1601

and,

NCDA Plant Conservation Program
P. O. Box 27647
Raleigh, N. C. 27611
(919) 733-3610

2. Description of any streams or wetlands affected by the project. If applicable, include the linear feet of stream that will be channelized or relocated.
3. Cover type maps showing wetland acreage impacted by the project. Wetland acreage should include all project-related areas that may undergo hydrologic change as a result of ditching, other drainage, or filling for project construction. Wetland identification may be accomplished through coordination with the U. S. Army Corps of Engineers (USACE). If the USACE is not consulted, the person delineating wetlands should be identified and criteria listed.

4. Cover type maps showing acreage of upland wildlife habitat impacted by the proposed project. Potential borrow sites and waste areas should be included.
5. Show the extent to which the project will result in loss, degradation, or fragmentation of wildlife habitat (wetlands or uplands).
6. Include the mitigation plan for avoiding, minimizing or compensating for direct and indirect degradation in habitat quality as well as quantitative losses.
7. Address the overall environmental effects of the project construction and quantify the contribution of this individual project to environmental degradation.
8. Provide a discussion of the probable impacts on natural resources, which will result from secondary development, facilitated by the improved road access.
9. If construction of this facility is to be coordinated with other state, municipal, or private development projects, a description of these projects should be included in the environmental document, and all project sponsors should be identified.

Thank you for the opportunity to provide input in the early planning stages of this project. If you have any questions regarding these comments, please contact me at (704) 485-8291.

cc: Mike Parker, NCDWQ



North Carolina Department of Crime Control and Public Safety
 Division of Emergency Management
 Office of Geospatial and Technology Management

Beverly Eaves Perdue, Governor
 Reuben F. Young, Secretary

H. Douglas Hoell, Jr., Director

May 26, 2011

State Clearinghouse
 N.C. Department of Administration
 1301 Mail Service Center
 Raleigh, North Carolina 27699-1301



Subject: Intergovernmental Review State Number: 11-E-0000-0252
 Wilma Dykeman Riverway Project – U-5019

As requested by the North Carolina State Clearinghouse, the North Carolina Department of Crime Control and Public Safety Division of Emergency Management Office of Geospatial and Technology Management (GTM) reviewed the proposed project listed above and offer the following comments:

- 1) The City of Asheville participates in the National Flood Insurance Program and enforces a Flood Damage Prevention Ordinance that requires a Floodplain Development Permit be issued for all development located in the SFHA within their jurisdiction. Please ensure the Floodplain Administrator reviews and issues a permit for the proposed project.

- 2) The proposed 2.2 mile section of the 17 mile Wilma Dykeman Riverway Corridor includes encroachments into the floodway of the French Broad and Swannanoa Rivers. The City of Asheville’s ordinance requires a flood study to ensure the proposed project does not increase the flood levels during the occurrence of the base flood discharge. This study is referred to as a no-rise certification. If a no-rise certification cannot be obtained and the City of Asheville intends to continue with the project a Conditional Letter of Map Revision (CLOMR) must be requested and obtained through FEMA before a floodplain development permit may be issued. In addition, Executive Order 11988 directs projects that are federally funded or financed to comply with provisions identified in Federal, State or local floodplain regulations.

Mail:
 4719 Mail Service Center
 Raleigh, NC 27699-4719
 Telephone: 919-715-5711



Location:
 1812 Tillery Place, Suite 105
 Raleigh, NC 27604
 Fax: 919-715-0408

www.NCCrimeControl.org

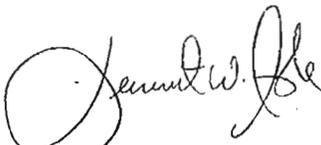
An Equal Opportunity/Affirmative Action Employer

A Nationally Accredited Agency

- 3) Records show 119 Riverside Drive was included in a mitigation buyout program. Certain deed restrictions may prohibit additional impervious surfaces to be added to this lot. A detailed deed review and coordination with the NC State Hazard Mitigation Officer, Chris Crew, at 919-715-8000 is suggested to determine permitted uses for this property and avoid unexpected delays.

Thank you for your cooperation and consideration. If you have any questions concerning the above comments, please contact John Gerber, P.E., CFM, the NC NFIP State Coordinator at (919) 715-5711, by email at jgerber@ncem.org or at the address shown on the footer of this documents.

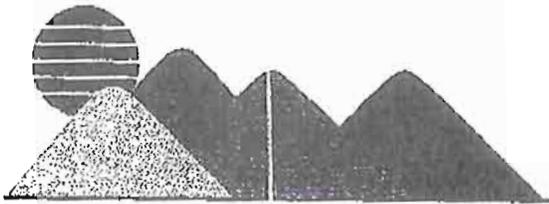
Sincerely,



Kenneth W. Ashe, P.E., CFM
Assistant Director

c: John Gerber, NFIP State Coordinator





Land-of-Sky Regional Council

Buncombe • Henderson • Madison • Transylvania Counties

Regional Clearinghouse

N. C. Intergovernmental Review Process Review and Comment Form

The Land-of-Sky Regional Council has received the attached information about a proposal which could affect your jurisdiction.

If you need more information, contact the applicant directly.

If you wish to comment on this proposed action, complete this form and return it with your comments to this office by 5/25/2011. Comments received after this date cannot be included in our response to the State Clearinghouse.

If you need additional time in order to obtain more information about the application, to formulate your comments, please call Michelle Barber at 828/251-6622 as soon as possible. An extension of the review period may be possible.

A NOTE to Reviewers - Projects with a "C" in the State Application Identifier (below) is a funding proposal review. Comments should focus on the acceptability or unacceptability of the project. Projects with an "E" in the identifier are environmental or site reviews. Comments for these projects should focus on the adequacy of the environmental document or site selection process.

If no comment is received by the above date, it will be assumed you have no comments regarding this proposal.

State Application Identifier # 11-E-0000-0252 Regional # 10-2011
Commenter's Name Ben Townley Title Director, LGS
Representing Land-of-Sky Regional Council
(Local Government)

Address _____

Phone _____ Date _____

Comment (or attach): See attached from qualified staff of LORSRC





North Carolina Department of Environment and Natural Resources

Beverly Eaves Perdue
Governor

Dee Freeman
Secretary

MEMORANDUM

TO: Sheila Green
State Clearinghouse

FROM: Melba McGee ✓
Environmental Review Coordinator

RE: 11-0252 Scoping - Wilma Dykeman Riverway Project in Buncombe
County along the French Broad and Swannanoa Rivers

DATE: June 2, 2011

The Department of Environment and Natural Resources has reviewed the proposed project. The attached comments are for the applicant's consideration. More specific comments will be provided during the environmental review process.

Thank you for the opportunity to respond.

Attachments



North Carolina Department of Administration

Beverly Eaves Perdue, Governor

Moses Carey, Jr., Secretary

June 6, 2011

Mr. Dan Baechtold
City of Asheville
Transportation Department
P.O. Box 7148
Asheville, NC 28802

Dear Mr. Baechtold:

Re: SCH File # 11-E-0000-0252; SCOPING; Proposal of the U-5019, Wilma Dykeman Riverway project development will focus on a 2.2 mile section of roadway including the corridor of Lyman Street and a portion of Riverside Drive from Amboy Road (SR 3556) to Hill Street (SR 1231)

The above referenced environmental impact information has been submitted to the State Clearinghouse under the provisions of the National Environmental Policy Act. According to G.S. 113A-10, when a state agency is required to prepare an environmental document under the provisions of federal law, the environmental document meets the provisions of the State Environmental Policy Act. Attached to this letter for your consideration are the comments made by agencies in the course of this review.

If any further environmental review documents are prepared for this project, they should be forwarded to this office for intergovernmental review.

Should you have any questions, please do not hesitate to call.

Sincerely,

A handwritten signature in black ink, appearing to read "Sheila Green".

Sheila Green
State Environmental Review Clearinghouse

Attachments

cc: Region B

Mailing Address:
1301 Mail Service Center
Raleigh, NC 27699-1301

Telephone: (919)807-2425
Fax (919)733-9571
State Courier #51-01-00
e-mail state.clearinghouse@doa.nc.gov

Location Address:
116 West Jones Street
Raleigh, North Carolina



North Carolina Department of Environment and Natural Resources

Division of Water Quality
Colleen H. Sullins
Director

Beverly Eaves Perdue
Governor

Dee Freeman
Secretary

June 9, 2011

MEMORANDUM

To: Melba McGee, Environmental Coordinator, Office of Legislative and Intergovernmental Affairs

From: Brian Wrenn, NC Division of Water Quality, Transportation Permitting Unit *BW*

Subject: Scoping comments on the Wilma Dykeman Riverway Project in Buncombe County, State
Project No. WBS No. 41503.1.1, TIP U-5019, State Clearinghouse Project No. 11-0252

Reference your correspondence dated April 15, 2011 in which you requested comments for the referenced project. Preliminary analysis of the project reveals the potential for multiple impacts to streams and jurisdictional wetlands in the project area. More specifically, impacts to:

Stream Name	River Basin	Stream Classification(s)	Stream Index Number	303(d) Listing
French Broad River	French Broad	B	6-(54.5)	No
Swannanoa River	French Broad	C	6-78	No

Further investigations at a higher resolution should be undertaken to verify the presence of other streams and/or jurisdictional wetlands in the area. In the event that any jurisdictional areas are identified, the Division of Water Quality requests that City of Asheville consider the following environmental issues for the proposed project:

Project Specific Comments:

1. The referenced document states that the project "is a proposed 2.2 mile section of the 17 mile Wilma Dykeman Riverway corridor." Any environmental assessment developed should consider the entire Wilma Dykeman Riverway corridor. Evaluating the corridor section by section limits the amount of avoidance and minimization that can occur on future portions of the project. A significant legal record exists regarding consideration of transportation corridors as a whole rather than section by section. Furthermore, NCDOT develops environmental documents for transportation projects by evaluating the entire corridor.
2. The referenced document states that "The City of Asheville is initiating the preparation of an Environmental Assessment/Finding of No Significant Impact for the River Arts District Transportation Project." The document goes on to say that six alternatives have been developed for the project. However, no purpose and need statement is included in the documentation. It seems out

Transportation Permitting Unit
1650 Mail Service Center, Raleigh, North Carolina 27699-1650
Location: 2321 Crabtree Blvd., Raleigh, North Carolina 27604
Phone: 919-733-4786 \ FAX: 919-733-6893
Internet: <http://h2o.enr.state.nc.us/nw/welland/sf>



of the NEPA sequence to present alternatives before a purpose and need statement has been developed or presented. Please provide a purpose and need statement for the project.

3. Although much of the developable land shown in the corridor is existing impervious surface from past businesses, NCDWQ will be very interested in the results of the indirect and cumulative impact analysis for this project. Existing sewer and water service combined with river front views will make development opportunities along this riverway very attractive.

General Project Comments:

1. The environmental document shall provide a detailed and itemized presentation of the proposed impacts to wetlands and streams with corresponding mapping. If mitigation is necessary as required by 15A NCAC 2H.0506(h), it is preferable to present a conceptual (if not finalized) mitigation plan with the environmental documentation. Appropriate mitigation plans will be required prior to issuance of a 401 Water Quality Certification.
2. Environmental assessment alternatives shall consider design criteria that reduce the impacts to streams and wetlands from storm water runoff. These alternatives shall include road designs that allow for treatment of the storm water runoff through best management practices as detailed in the most recent version of NCDWQ *Stormwater Best Management Practices*, such as grassed swales, buffer areas, preformed scour holes, retention basins, etc.
3. After the selection of the preferred alternative and prior to an issuance of the 401 Water Quality Certification, City of Asheville is respectfully reminded that they will need to demonstrate the avoidance and minimization of impacts to wetlands (and streams) to the maximum extent practical. In accordance with the Environmental Management Commission's Rules {15A NCAC 2H.0506(h)}, mitigation will be required for impacts of greater than 1 acre to wetlands. In the event that mitigation is required, the mitigation plan shall be designed to replace appropriate lost functions and values. The NC Ecosystem Enhancement Program may be available for use as wetland mitigation.
4. In accordance with the Environmental Management Commission's Rules {15A NCAC 2H.0506(h)}, mitigation will be required for impacts of greater than 150 linear feet to any single stream. In the event that mitigation is required, the mitigation plan shall be designed to replace appropriate lost functions and values. The NC Ecosystem Enhancement Program may be available for use as stream mitigation.
5. NCDWQ is very concerned with sediment and erosion impacts that could result from this project. City of Asheville shall address these concerns by describing the potential impacts that may occur to the aquatic environments and any mitigating factors that would reduce the impacts.
6. If a bridge is being replaced with a hydraulic conveyance other than another bridge, NCDWQ believes the use of a Nationwide Permit may be required. Please contact the US Army Corp of Engineers to determine the required permit(s).
7. If the old bridge is removed, no discharge of bridge material into surface waters is allowed unless otherwise authorized by the US ACOE. Strict adherence to the Corps of Engineers guidelines for bridge demolition will be a condition of the 401 Water Quality Certification.
8. Whenever possible, NCDWQ prefers spanning structures. Spanning structures usually do not require work within the stream or grubbing of the streambanks and do not require stream channel realignment. The horizontal and vertical clearances provided by bridges shall allow for human and wildlife passage beneath the structure. Fish passage and navigation by canoeists and boaters shall not be blocked. Bridge supports (bents) shall not be placed in the stream when possible.

9. Bridge deck drains shall not discharge directly into the stream. Stormwater shall be directed across the bridge and pre-treated through site-appropriate means (grassed swales, pre-formed scour holes, vegetated buffers, etc.) before entering the stream. Please refer to the most current version of NCDWQ's *Stormwater Best Management Practices*.
10. If concrete is used during construction, a dry work area shall be maintained to prevent direct contact between curing concrete and stream water. Water that inadvertently contacts uncured concrete shall not be discharged to surface waters due to the potential for elevated pH and possible aquatic life and fish kills.
11. If temporary access roads or detours are constructed, the site shall be graded to its preconstruction contours and elevations. Disturbed areas shall be seeded or mulched to stabilize the soil and appropriate native woody species should be planted. When using temporary structures the area shall be cleared but not grubbed. Clearing the area with chain saws, mowers, bush-hogs, or other mechanized equipment and leaving the stumps and root mat intact allows the area to re-vegetate naturally and minimizes soil disturbance.
12. Placement of culverts and other structures in waters, streams, and wetlands shall be below the elevation of the streambed by one foot for all culverts with a diameter greater than 48 inches, and 20 percent of the culvert diameter for culverts having a diameter less than 48 inches, to allow low flow passage of water and aquatic life. Design and placement of culverts and other structures including temporary erosion control measures shall not be conducted in a manner that may result in disturbance of wetlands or streambeds or banks, adjacent to or upstream and down stream of the above structures. The applicant is required to provide evidence that the equilibrium is being maintained if requested in writing by NCDWQ. If this condition is unable to be met due to bedrock or other limiting features encountered during construction, please contact NCDWQ for guidance on how to proceed and to determine whether or not a permit modification will be required.
13. If multiple pipes or barrels are required, they shall be designed to mimic natural stream cross section as closely as possible including pipes or barrels at flood plain elevation, floodplain benches, and/or sills may be required where appropriate. Widening the stream channel shall be avoided. Stream channel widening at the inlet or outlet end of structures typically decreases water velocity causing sediment deposition that requires increased maintenance and disrupts aquatic life passage.
14. If foundation test borings are necessary; it should be noted in the document. Geotechnical work is approved under General 401 Certification Number 3624/Nationwide Permit No. 6 for Survey Activities.
15. Sediment and erosion control measures sufficient to protect water resources must be implemented and maintained in accordance with the most recent version of North Carolina Sediment and Erosion Control Planning and Design Manual and the most recent version of NCS000250.
16. All work in or adjacent to stream waters shall be conducted in a dry work area unless otherwise approved by NCDWQ. Approved BMP measures from the most current version of NCDOT Construction and Maintenance Activities manual such as sandbags, rock berms, cofferdams and other diversion structures should be used to prevent excavation in flowing water.
17. Sediment and erosion control measures shall not be placed in wetlands and streams.

18. Borrow/waste areas shall avoid wetlands to the maximum extent practical. Impacts to wetlands in borrow/waste areas could precipitate compensatory mitigation.
19. While the use of National Wetland Inventory (NWI) maps, NC Coastal Region Evaluation of Wetland Significance (NC-CREWS) maps and soil survey maps are useful tools, their inherent inaccuracies require that qualified personnel perform onsite wetland delineations prior to permit approval.
20. Heavy equipment shall be operated from the bank rather than in stream channels in order to minimize sedimentation and reduce the likelihood of introducing other pollutants into streams. This equipment shall be inspected daily and maintained to prevent contamination of surface waters from leaking fuels, lubricants, hydraulic fluids, or other toxic materials.
21. In most cases, NCDWQ prefers the replacement of the existing structure at the same location with road closure. If road closure is not feasible, a temporary detour should be designed and located to avoid wetland impacts, minimize the need for clearing and to avoid destabilizing stream banks. If the structure will be on a new alignment, the old structure shall be removed and the approach fills removed from the 100-year floodplain. Approach fills should be removed and restored to the natural ground elevation. The area shall be stabilized with grass and planted with native tree species. Tall fescue shall not be used in riparian areas.
22. Riprap shall not be placed in the active thalweg channel or placed in the streambed in a manner that precludes aquatic life passage. Bioengineering boulders or structures should be properly designed, sized and installed.

Thank you for requesting our input at this time. City of Asheville is reminded that issuance of a 401 Water Quality Certification requires that appropriate measures be instituted to ensure that water quality standards are met and designated uses are not degraded or lost. If you have any questions or require additional information, please contact Brian Wrenn at 919-807-6365.

cc: Lori Beckwith, US Army Corps of Engineers, Asheville Field Office
Chris Militscher, Environmental Protection Agency (electronic copy only)
Marla Chambers, NC Wildlife Resources Commission
Marella Buncick, US FWS (electronic copy only)
Mike Parker, NCDWQ Asheville Regional Office
File Copy



North Carolina Department of Environment and Natural Resources

Beverly Eaves Perdue
Governor

Dee Freeman
Secretary

MEMORANDUM

TO: Sheila Green
State Clearinghouse

FROM: Melba McGee 
Project Review Coordinator

RE: 11-0252 Scoping - Wilma Dykeman Riverway Project in Buncombe
County

DATE: June 15, 2011



The attached comments were received by this office after the response due date. These comments should be forwarded to the applicant and made a part of our previous comment package.

Thank you for the opportunity to respond.

Attachment



North Carolina Department of Cultural Resources

State Historic Preservation Office

Claudia Brown, Acting Administrator

Beverly Eaves Perdue, Governor
Linda A. Carlisle, Secretary
Jeffrey J. Crow, Deputy Secretary

Office of Archives and History
Division of Historical Resources
David Brook, Director

June 16, 2011

Rajit Ramkumar
Wilbur Smith Associates
421 Fayetteville Street, Suite 1303
Raleigh, NC 27601

Re: Wilma Dykeman Riverway Project, U-5019, Buncombe County, CH 11-0645

Dear Mr. Ramkumar:

Thank you for your letter of May 24, 2011, transmitting the archaeological survey report by Blue Ridge Archaeological Consultants. The report meets our guidelines and those of the Secretary of the Interior.

During the course of the survey, no archaeological sites were located within the project area. Shovel tests conducted by Blue Ridge Archaeological Consultants indicated that in most areas there was modern fill to a depth of a meter or more. The report authors have recommended that a professional archaeologist monitor any proposed ground disturbance that will extend to a depth of more than one meter, and that no further archaeological work be undertaken in areas where ground disturbance will be limited to a depth of less than one meter. We concur with these recommendations.

The above comments are made pursuant to Section 106 of the National Historic Preservation Act and the Advisory Council on Historic Preservation's Regulations for Compliance with Section 106 codified at 36 CFR Part 800.

Thank you for your cooperation and consideration. If you have questions concerning the above comment, please contact Renee Gledhill-Earley, environmental review coordinator, at 919-807-6579. In all future communication concerning this project, please cite the above-referenced tracking number.

Sincerely,

for Claudia Brown

cc: Dan Baechtold, City of Asheville
Scott Schumate, Blue Ridge Archaeological Consultants



North Carolina Department of Administration

Beverly Eaves Perdue, Governor

Moses Carey, Jr., Secretary

June 17, 2011

Mr. Dan Baechtold
City of Asheville
Transportation Department
P.O. Box 7148
Asheville, NC 28802

Dear Mr. Baechtold:

Re: SCH File # 11-E-0000-0252; SCOPING; Proposal of the U-5019, Wilma Dykeman Riverway project development will focus on a 2.2 mile section of roadway including the corridor of Lyman Street and a portion of Riverside Drive from Amboy Road (SR 3556) to Hill Street (SR 1231)

The above referenced environmental impact information has been submitted to the State Clearinghouse under the provisions of the National Environmental Policy Act. According to G.S. 113A-10, when a state agency is required to prepare an environmental document under the provisions of federal law, the environmental document meets the provisions of the State Environmental Policy Act. Attached to this letter for your consideration are **additional** comments made by agencies in the course of this review.

If any further environmental review documents are prepared for this project, they should be forwarded to this office for intergovernmental review.

Should you have any questions, please do not hesitate to call.

Sincerely,


Sheila Green
State Environmental Review Clearinghouse

Attachments

cc: Region B

Mailing Address:
1301 Mail Service Center
Raleigh, NC 27699-1301

Telephone: (919)807-2425
Fax (919)733-9571
State Courier #51-01-00
e-mail state.clearinghouse@doa.nc.gov

Location Address:
116 West Jones Street
Raleigh, North Carolina



North Carolina Department of Cultural Resources
State Historic Preservation Office
Claudia Brown, Acting Administrator

Beverly Eaves Perdue, Governor
Linda A. Carlisle, Secretary
Jeffrey J. Crow, Deputy Secretary

Office of Archives and History
Division of Historical Resources
David Brook, Director

June 21, 2011

Rajit Ramkumar
Wilbur Smith Associates
421 Fayetteville Street, Suite 1303
Raleigh, NC 27601

Re: Historic Architectural Resources Report, Wilma Dykeman Riverway, Asheville, U-5019,
Buncombe County, CH 11-0645

Dear Mr. Ramkumar:

Thank you for your letter of May 23, 2011, transmitting the architectural survey report for the above project.

All properties included in this survey must have a survey site number. Please contact Chandrea Burch at 919-807-7286 to obtain these numbers; use the existing numbers given below for previously surveyed sites, such as the Hans Ree Tannery and the Asheville Cotton Mill. After these site numbers are obtained, please submit a list containing the name, survey ID letter, and survey site number for each property, i.e., "Asheville Cotton Mill (Remnant Structures): BN 0229, Property H." Please include the survey site number for each property on any additional or revised submittals.

For the purposes of compliance with Section 106 of the National Historic Preservation Act, we concur that the following properties are listed in, and remain eligible for, the National Register of Historic Places, as contributing resources within the Riverside Industrial Historic District:

- ◆ Asheville Cotton Mill Cloth Warehouse (Property K);
- ◆ Leemon Distributing Company Warehouse (Property L);
- ◆ Storage Supply Company (Property M);
- ◆ Kent Building (Property N);
- ◆ Pearce-Young-Angel Company (Property O);
- ◆ S. Sternberg Company (Property Q);
- ◆ Farmers Federation Building (Property R);
- ◆ Storage Warehouse (Property S);
- ◆ Standard Oil Company (Property T), including the Office, Garage, Pump Building, Oil Warehouse, and Storage Building;
- ◆ American Feed Milling Company (Property U)
- ◆ Asheville Mica Company/Carolina Coal and Ice Company (Property V);
- ◆ Scale Office (Carolina Coal and Ice Company) (Property W); and,
- ◆ Post Machinery Company Machine Shop (Property X).

We concur that the following properties are eligible for listing in the National Register under the criteria cited:

- ◆ **Southern Railroad Bridge** across the French Broad River (Property A): Criterion A for its association with the history of travel and tourism in western North Carolina and Criterion C for its design and construction;
- ◆ **McKinney Welding Supply Company Building** (Property F): Criterion A for its association with several schools and organizations and Criterion C for its design and construction;
- ◆ **Texas Oil Company Buildings** (Property Y): Criterion C for their design and construction. However, we cannot concur with the determination that the property is eligible under Criterion A based on the evidence provided; and,
- ◆ **Hans Ree Tannery** (BN 0414, Properties AC, AD, and AE): Criteria A for its association with the tanning industry and Criterion C for its design and construction. These sites should be considered together and the National Register boundary should include the four parcels south of Day's Tobacco Warehouse, west of the railroad, and north and east of Lyman Street (exclusive of the Inspection Station parcel (Property ddd) and the parcel containing the modern warehouse at the south end of the site). Buildings 1, 3, and 4 and rear buildings 1, 2, 5, 6, 10, 11, and 12 are contributing resources, while the modern warehouse at the north end of the property is non-contributing.

We concur that the following properties are *not* eligible for listing in the National Register:

- ◆ **Asheville Auto Parts, Buildings 1, 2, and 3** (Property aaa);
- ◆ **455 Riverside Drive, Building 2** (Property bbb);
- ◆ **Smith Bridge** (Haywood/Craven Street Bridge) (Property E);
- ◆ **West Asheville Bridge** (Riverlink Bridge) (Property P);
- ◆ **12 Bones Restaurant** (Property ccc); and,
- ◆ **Inspection Station and Truck Repair** (Property ddd).

We do not concur with the finding that the **Southern Coal Company Buildings** (Property B) are eligible for listing in the National Register. There is not enough evidence provided to support the claim that these buildings "have made a significant contribution" to the history of the area, and the buildings are clearly associated with a larger industrial complex that does not appear to exist presently. Without this complex, these buildings lack the historical context and integrity of setting and association required for eligibility.

We do not concur with the finding that the "**Old**" **Smoky Park Highway Bridge** (Property C) is not eligible for listing in the National Register. The bridge was determined *eligible* for listing in the National Register under Criterion C for its design by the North Carolina Department of Transportation (NCDOT) in 2005 as part of its Historic Bridge Inventory. Thus, it remains eligible for the Register, barring information that it or its integrity has changed since 2005.

We do not concur with the finding that the **Henry J. Olive General Merchandise/J. M. Westall Lumber Company Building** (Property D) is eligible for listing in the National Register. Per the 1917 Sanborn maps, the Westall Lumber Company operated on what is now the block bound by Haywood, Riverside, Patton Avenue/I-240, and the railroad tracks. Do the buildings on Haywood, adjacent to the corner brick building, have any historical association with the Westall/Olive businesses? If so, all of these buildings need to be evaluated together as one site. If not, does the corner building alone possess sufficient significance and integrity to meet Criteria A and C?

We do not concur with the finding that the **Cone Mills Corporate Office** (Property G), the **Asheville Cotton Mill (Remnant Structures)** (Property H, BN 0229), and the **Chesterfield Mill Site** (Property I) are eligible for listing in the National Register. Page 36 of the report shows that the Asheville Cotton Mill was owned and operated by the Cone family from 1883 to 1949; presumably, the Corporate Office was related to the adjacent mill. If so, the two sites should be evaluated together. The cotton mill was placed on the State Study List—a preliminary step in the review of potential nominations to the National Register of Historic Places—in 1980, but burned in 1995. Because of this loss of integrity, in 2004, when the Riverside Industrial Historic District was listed in the National Register, the mill was excluded from the district. Likewise, the Chesterfield Mill Site was also excluded from the district based upon its loss of integrity.

We do not concur with the finding that the **Two Mill Houses** (Property J) are eligible for listing in the National Register. Altered mill houses have typically not been determined individually eligible for the National Register.

We do not concur with the finding that the **J. A. Baker Packing Company** (Property Z) is eligible for listing in the National Register. This site is heavily altered, and the few remaining circa 1925 features have been dwarfed by the large modern additions to the south and east.

We do not concur with the finding that the **Day's Tobacco Warehouse** (Property AA) is eligible for listing in the National Register. This site appears to be heavily altered and the large addition to the south has compromised the site's integrity.

We do not concur with the finding that the **Railroad Utility Building** (Property AB) is eligible for listing in the National Register. This building appears to be an accessory building to the Southern Railway Rail Yard and Roundhouse. The Roundhouse has since been demolished, and the Rail Yard has been heavily altered and many of the spur lines have been removed. Although the Utility Building appears unaltered, without the Roundhouse and Rail Yard it loses much of its historical context and integrity of setting and association required for eligibility.

We cannot concur with the finding that the **Carrier Bridge** (Property AF) is eligible for listing in the National Register, because the bridge was not determined eligible for listing in the National Register by NCDOT in 2005.

If you do not agree with the above findings, please address the above questions and concerns by providing additional information to make a stronger, more substantial case for their eligibility.

The above comments are made pursuant to Section 106 of the National Historic Preservation Act and the Advisory Council on Historic Preservation's Regulations for Compliance with Section 106 codified at 36 CFR Part 800.

Thank you for your cooperation and consideration. If you have questions concerning the above comment, please contact Renee Gledhill-Earley, environmental review coordinator, at 919-807-6579. In all future communication concerning this project, please cite the above-referenced tracking number.

Sincerely,



for Claudia Brown

cc: Stacy Merten, smerten@ashevillenc.gov



North Carolina Department of Cultural Resources

State Historic Preservation Office

Ramona M. Bartos, Administrator

Beverly Eaves Perdue, Governor
Linda A. Carlisle, Secretary
Jeffrey J. Crow, Deputy Secretary

Office of Archives and History
Division of Historical Resources
David Brook, Director

September 8, 2011

Rajit Ramkumar
Wilbur Smith Associates
421 Fayetteville Street
Suite 1303
Raleigh, NC 27601



Re: Wilma Dykeman Riverway, Asheville, TIP No. U-5019, Buncombe County, CH 11-0645

Dear Mr. Ramkumar:

Thank you for your letter of August 16, 2011, concerning the above project.

The project currently includes six proposed alternative alignments, all along the eastern bank of the French Broad River, from north of Hill Street to south of Amboy Street. The project's Area of Potential Effect includes the Riverside Industrial Historic District, which is listed in the National Register of Historic Places, and several other sites determined eligible for listing in the National Register in the historic architectural resources report.

Before we can fully comment on any potential effects of the project, please submit plans clearly showing the existing and proposed right-of-way, pavement (including sidewalks and multi-use paths), landscaping, and building footprints. Full-size plans in PDF format may be e-mailed to Environmental.Review@ncdcr.gov.

Based on the submitted plans, we offer the following preliminary comments:

Purple Route

The Purple Route appears to propose the demolition, in whole or in part, of the Asheville Mica Company/Carolina Coal and Ice Company (Property V in the historic architectural resources report), the Scale Office (Property W), and the Hans Ree Tannery (Properties AC, AD, and AE). The Asheville Mica Company and Scale Office are each contributing resources within the historic district; the Hans Ree Tannery has been determined eligible for listing in the National Register. Demolition of one or more of these buildings will have an adverse effect on historic properties.

Blue Route

The Blue Route appears to propose the demolition, in whole or in part, of the Asheville Mica Company/Carolina Coal and Ice Company (Property V) and the Scale Office (Property W). Demolition of one or more of these buildings will have an adverse effect on historic properties.

Cyan Route

The Cyan Route appears to propose the demolition, in whole or in part, of the Asheville Mica Company/Carolina Coal and Ice Company (Property V), the Scale Office (Property W), the Post Machinery Company Machine Shop (Property X), and the Hans Ree Tannery (Properties AC, AD, and AE). The Post Machinery Company Machine Shop is a contributing resource within the historic district. Demolition of one or more of these buildings will have an adverse effect on historic properties.

Green Route

The Green Route appears to propose the demolition, in whole or in part, of the Asheville Mica Company/Carolina Coal and Ice Company (Property V), the Scale Office (Property W), and the Post Machinery Company Machine Shop (Property X). Demolition of one or more of these buildings will have an adverse effect on historic properties.

Yellow Route

The Yellow Route appears to propose the demolition, in whole or in part, of the Asheville Mica Company/Carolina Coal and Ice Company (Property V), the Scale Office (Property W), and the Post Machinery Company Machine Shop (Property X). Demolition of one or more of these buildings will have an adverse effect on historic properties.

Orange Route

The Orange Route appears to propose the demolition, in whole or in part, of the Asheville Mica Company/Carolina Coal and Ice Company (Property V) and the Scale Office (Property W). Demolition of one or more of these buildings will have an adverse effect on historic properties.

No assessment of the potential effects to the Henry J. Olive General Merchandise/J. M. Westall Lumber Company Building (Property D) can be made until the questions raised in our letter of June 21, 2011 are addressed.

The above comments are made pursuant to Section 106 of the National Historic Preservation Act and the Advisory Council on Historic Preservation's Regulations for Compliance with Section 106 codified at 36 CFR Part 800.

Thank you for your cooperation and consideration. If you have questions concerning the above comment, please contact Renee Gledhill-Earley, environmental review coordinator, at 919-807-6579. In all future communication concerning this project, please cite the above-referenced tracking number.

Sincerely,



for Ramona M. Bartos

cc: Stacy Merten, Asheville & Buncombe County HRC, smerten@ashevillenc.gov

From: Michael.Batuzich@dot.gov
Sent: Friday, April 27, 2012 10:31 AM
To: RussellT@nc-chokeee.com; tylehowe@nc-chokeee.com
Cc: Thompson, Rebecca; Ramkumar, Rajit
Subject: FW: Section 106 kickoff for River Arts District TIP/U-5019

Russell and Tyler:

The Project Team for the above-referenced project is starting the Section 106 process for the River Arts District Transportation Improvement Project (NCDOT TIP U-5019) in Asheville, NC (see the project website at <http://www.ashevillenc.gov/Departments/Transportation/RADTransportationProject.aspx>). **The purpose of this email is to invite the EBCI THPO to participate on this project as a Section 106 Consulting Party.**

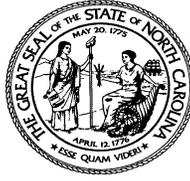
Section 106 of the National Historic Preservation Act of 1966, as amended (36 CFR 800), requires federal agencies to take into account the effects of their undertakings on historic properties (including archaeological sites). As part of the process, the project team will work through a three-step process with consulting parties to:

1. Identify historic properties that could be potentially affected by the project,
2. Assess project effects on these resources, and
3. Develop ways to avoid, minimize, or mitigate adverse effects on historic properties.

The project team will circulate draft materials and host a meeting/webinar this summer to discuss these three elements of the consultation process. Participation is voluntary and open to anyone with “with a demonstrated interest in the effect of the undertaking on properties listed on or eligible for listing on the Nation Register of Historic Places.” This may be business owners, historic preservation groups, neighborhood associations, or others who are interested. Additional information about the consultation process is available online at <http://www.achp.gov/citizensguide.html>.

If you would like to participate in the Section 106 process for the River Arts District Transportation Project, please respond to this email by May 25, 2012. If you do decide to participate, the project team will send you more information as the project progresses. Even if you don’t choose to participate, the information will be included in the environmental report for the project. If you have any questions or would like additional information, please contact Rebecca Thompson at ThompsonRD@cdmsmith.com.

Thank you.



**North Carolina Department of Cultural Resources
State Historic Preservation Office**

Ramona M. Bartos, Administrator

Beverly Eaves Perdue, Governor
Linda A. Carlisle, Secretary
Jeffrey J. Crow, Deputy Secretary

Office of Archives and History
Division of Historical Resources
David Brook, Director

June 11, 2012

Rebecca Thompson
CDM Smith
5400 Glenwood Avenue, Suite 300
Raleigh, NC 27612

Re: Draft Historic Architectural Resources Report, River Arts District Transportation Project,
Asheville, U-5019, Buncombe County, CH 11-0645

Dear Ms. Thompson:

Thank you for your e-mail of May 21, 2012, transmitting the above draft architectural survey report as prepared for the City of Asheville by Mathews Architecture and CDM Smith. We offer the following comments:

General

When evaluating properties, "The significance of a historic property can be judged and explained only when it is evaluated within its historic context."¹ Although the historic, architectural, or thematic contexts need not be reinvented (much of the context developed for, and included in, the Riverside Industrial Historic District National Register nomination is likely applicable here too), they must be incorporated here and each property evaluated against them as appropriate.

For any properties recommended as eligible for listing in the National Register, the evaluation must include a description of the proposed National Register boundary and a boundary justification. This justification must "explain how the boundaries were selected," and "clarify any issues that might raise questions, such as excluding portions of the historic property because of lost integrity."² When the proposed National Register boundary corresponds to existing property lines, please reference the parcel identification number and/or lot and block number.

Each visible elevation and any unique or architecturally significant features should be included in the description. Any information about the interior gained from site visits, photographs, interviews, or other resources should be included as well.

¹ Staff of the National Register of Historic Places. *National Register Bulletin: How to Apply the National Register Criteria for Evaluation*. National Park Service, 2002, 7.

² Seifert, Donne J. *National Register Bulletin: Defining Boundaries for National Register Properties*. National Park Service, 1997, 6.

For each property, photographs of each visible elevation should be included, as well as photographs showing details of construction, design, and any unique or architecturally significant exterior or interior elements. Photographs should be formatted such that they appear no smaller than 4" by 3" in the report. For more information on photograph requirements, including file naming conventions for the CD to be submitted with the final report, see Chapter 6 of the NCSHPO's [Architectural Survey Manual](#).

Include a detailed site plan and/or aerial photograph in each property's evaluation and show any proposed National Register boundaries. An overall area map of the street network and parcels, in addition to the area aerial photograph, would be helpful.

Include a comprehensive bibliography and appropriate citations throughout the report as necessary.

Were the interiors accessed? If so, a written description of the interior's current condition, any information about its historic condition, and photographs should be included in this report. Interiors should be considered when evaluating the property's integrity. If interiors have not been accessed, property owners should be contacted; if interior access is denied, note this in the report.

Include the preparer(s) professional qualifications and/or resume as an appendix to the report.

Please add the newly assigned survey site numbers throughout the report. In general, it is preferred that each site is referred to by a name instead of just an address, even if it is as generic as "house" or "commercial building."

Chapter 1: Introduction

Adverse effects may also include effects that are cumulative in nature; please revise the last sentence of the first paragraph of page 6 accordingly.

A finding of "no historic properties affected" is reached when there are no historic properties identified within the Area of Potential Effect (APE); please revise the second paragraph of page 6 accordingly.

Chapter 2: Area of Potential Effects

The APE as defined here is incorrect. In 36 C.F.R. §800.16(d) the APE is defined as "the geographic area or areas within which an undertaking may directly *or indirectly* [emphasis added] cause alterations in the character or use of historic properties, if any such properties exist." Additional types of adverse effects (beyond direct effects and visual effects) are identified in 36 C.F.R. §800.5(a)(2). Please revise this chapter accordingly.

The second paragraph of Chapter 2 should be revised to read, "...the APE follows the centerline of the French Broad River."

Riverside Industrial Historic District (BN 1827)

Even though this historic district is presently listed in the National Register, this report must contain a description, historical background information, an evaluation using the National Register criteria, an evaluation of the district's historic integrity, and a description and justification of the National Register boundaries and any proposed changes thereto.

Is it more appropriate to consider the Texas Oil Company Buildings as potential contributing buildings to an expanded Riverside Industrial Historic District, rather than evaluating them individually?

Southern Railroad Bridge (BN 5928)

Is this bridge currently in use? Has it been altered at all since 1910? Did Riverside Drive and Emma Road exist when the bridge was built, or were the overpasses added later?

Please add the proposed linear limits of the National Register boundary—does it include any abutments or other ancillary features?

Old Smoky Park Highway Bridge (BN 2469)

For a thorough description of the bridge, please see NCDOT's *Historic Bridge Inventory*.

By "first major automobile crossing" is it meant that this was the first *highway* bridge across the French Broad River? If it can be shown that the bridge played a significant role in the development of West Asheville, the bridge may be eligible under Criterion A as well.

McKinney Welding Supply Company (BN 0530)

The site should be referred to using the name that "best reflects the property's historic importance or was commonly used for the property during the period of significance."³ No reference to the McKinney Welding Supply Company is made in the description or evaluation.

The Buncombe County tax records for this parcel show a construction date of 1940. A building first appears on this site on the 1896 Sanborn map. However, it does not seem to be the same as the existing building. The 1907 Sanborn map indicates a club room is present on the second floor, with interior stairs located at the center-front of the building. Please confirm the construction date and provide supporting evidence. At the very least, the mission-style parapet seems unlikely to date from the turn of the twentieth century.

Provide more history on the clubs and schools that used the building and their relation to the surrounding mills. Why were these institutions significant?

Note specifically that the concrete block addition is a non-contributing addition.

Texas Oil Company Buildings (BN 5929)

The awning on the office should be described. Is it possible to discern when this was built/added?

The 1917 Sanborn map and the Buncombe County tax records show both buildings dating to 1916.

Is any information available about the oil tanks formerly located on the north side of the garage?

A gable-roofed addition to the rear of the garage is visible in aerial photographs. This addition should be included in the site description.

Glen Rock Hotel (BN0400)

In 2009, Mountain Housing Opportunities (MHO) submitted to the National Park Service (NPS) a draft National Register nomination as part of the Historic Preservation Certification Application (Part I of the federal historic rehabilitation tax credit review process). At that time, NPS determined that the Glen Rock Hotel in its present state was *not* eligible for listing in the National Register as the historic integrity was severely compromised by the in-filled storefronts. NPS and SHPO staff advised MHO that the building would only be eligible for listing if these storefronts were re-opened in a manner consistent with the *Secretary of the Interior's Standards for Rehabilitation*.

³ Staff of the National Register of Historic Places. *National Register Bulletin: How to Complete the National Register Registration Form*. National Park Service, 1997, 8.

In 2011, we were notified by MHO that the project was no longer seeking historic rehabilitation tax credits, but that they would be utilizing federal funding. As such, we reviewed the project for compliance with Section 106 and again determined that the building was not eligible. The latest rehabilitation plans we have seen showed the in-filled storefronts to be removed and replaced with modern materials and in a design inconsistent with the *Standards*; thus, the property would remain ineligible after the conclusion of the project.

The storefronts, in their present condition, should be included in the site description and the discussion of the site's historic integrity. What remains of the storefronts on the interior behind the plywood and concrete block?

Typically only the home or studio of a prominent architect is eligible under Criterion B as they "usually are the properties with which they are most personally associated."⁴

Hans Ree Tannery (BN 0414)

All of the buildings and features of the site need to be included in the description. Several large metal-roofed buildings along the railroad tracks, the central storage yard, and the collapsing building northeast of the office need to be included in the description.

Include a description of the tanning process, what role each building played, tanning's role (and this facility's role in particular) in Asheville's history and economy, and why the tannery located here.

The present use of the site and/or individual buildings should be included.

Norfolk-Southern Roundhouse

Aerial photographs appear to show a 25-stall roundhouse, composed of an 18-stall section (all of which have been closed in with windows removed) with a 7-stall section (all open and currently in operation). Is this 7-stall section an addition, or is it original and just happens to be the only part that has been rehabilitated/maintained?

The rectangular wing on the north side of the roundhouse needs to be described and evaluated.

J. M. Westall Lumber Company Building (BN 0339)

The first draft of the architectural survey refers to this site as the "(Former) Henry J. Olive General Merchandise/J. M. Westall Lumber Company Building." Is the Olive name still applicable? If so, it should be included in the site history.

Other Properties

Is the Depot Street bridge over Nasty Branch (just north of the Glen Rock Hotel) fifty years old? If so, it should be included in the report.

When were the additions to the **J. A. Baker Packing Company** building made? Provide a description of these alterations, and what, if any, of the original structure and footprint remain.

What remains behind the awning at **362 Depot Street**? The awning is likely a reversible alteration, and the changes to storefront and replacement windows do not appear to severely compromise the building's integrity.

Based on aerial photographs, it appears that the art deco storefront at **375 Depot Street** was added on to an existing gable-front building. Please provide more information on the evolution of this building and its additions. The art deco additions may have their own significance now.

⁴ *National Register Bulletin: How to Apply the National Register Criteria for Evaluation*, 16.

Ineligible Properties

For the purpose of compliance with Section 106 of the National Historic Preservation Act, we concur that the following properties are *not* eligible for listing in the National Register:

- ◆ **Asheville Auto Parts Buildings** (BN 5930);
- ◆ **455 Riverside Drive** (BN 5931);
- ◆ **233 Riverside Drive** (BN 5934);
- ◆ **151 West Haywood Street;**
- ◆ **Asheville Cotton Mill** (remnants) (BN 0229);
- ◆ **Cone Mills Office** (BN 3846);
- ◆ **Earle-Chesterfield Mill and Feed Company;**
- ◆ **Mill Workers Houses** (BN 5936 and BN 5937);
- ◆ **Grey Eagle Tavern;**
- ◆ **Park Avenue Bridge;**
- ◆ **163 Park Avenue;**
- ◆ **200-220 Clingman Avenue;**
- ◆ **121 Lyman Street;**
- ◆ **12 Bones Restaurant** (BN 5932);
- ◆ **270 Depot Street;**
- ◆ **290 Depot Street;**
- ◆ **342-348 Depot Street;**
- ◆ **347 Depot Street;**
- ◆ **National Biscuit Company;**
- ◆ **351 Depot Street;**
- ◆ **352 Depot Street;**
- ◆ **357 Depot Street;**
- ◆ **Day's Tobacco Warehouse** (BN 0358);
- ◆ **Railroad Utility Building** (BN 5939);
- ◆ **Truck Repair Building** (BN 5933); and,
- ◆ **Carrier Bridge** (BN 5940).

The above comments are made pursuant to Section 106 of the National Historic Preservation Act and the Advisory Council on Historic Preservation's Regulations for Compliance with Section 106 codified at 36 CFR Part 800.

Thank you for your cooperation and consideration. If you have questions concerning the above comment, please contact Renee Gledhill-Earley, environmental review coordinator, at 919-807-6579. In all future communication concerning this project, please cite the above-referenced tracking number.

Sincerely,



for Ramona M. Bartos

cc: Dan Baechtold, City of Asheville, dbaechtold@ashevillenc.gov
Robert Ball, CDM Smith, ballrw@cdmsmith.com
Zahid Baloch, NC DOT, zbaloch@ncdot.gov
Michael Batuzich, FHWA, Michael.Batuzich@dot.gov
Mary Pope Furr, NC DOT, mfurr@ncdot.gov
Stacy Merten, Asheville & Buncombe County HRC, smerten@ashevillenc.gov
Rajit Ramkumar, CDM Smith, ramkumarr@cdmsmith.com



North Carolina Department of Cultural Resources State Historic Preservation Office

Ramona M. Bartos, Administrator

Beverly Eaves Perdue, Governor
Linda A. Carlisle, Secretary
Jeffrey J. Crow, Deputy Secretary

Office of Archives and History
Division of Historical Resources
David Brook, Director

July 24, 2012

Rebecca Thompson
CDM Smith
5400 Glenwood Avenue, Suite 300
Raleigh, NC 27612

Re: Draft Historic Architectural Resources Report, River Arts District Transportation Project,
Wilma Dykeman Riverway Project, Asheville, U-5019, Buncombe County, CH 11-0645

Dear Ms. Thompson:

Thank you for your e-mail of July 10, 2012, transmitting the above revised draft architectural survey report prepared by Mathews Architecture and CDM Smith for the City of Asheville.

For the purposes of compliance with Section 106 of the National Historic Preservation Act, we concur that the **Riverside Industrial Historic District** (BN 1827) is currently listed in, and remains eligible for listing in, the National Register of Historic Places under Criterion A for its association with commerce and industry and under Criterion C for its architecture. The proposed boundary expansion to include the **Texas Oil Company** (BN 5929) appears appropriate.

For the purposes of compliance with Section 106 of the National Historic Preservation Act, we concur that the following properties are eligible for listing in the National Register under the criteria cited, and that the proposed National Register boundaries appear appropriate:

- ◆ **Southern Railroad Bridge** (BN 5928), Criteria A and C;
- ◆ **Old Smoky Park Highway Bridge** (BN 2469), Criteria A and C;
- ◆ **Texas Oil Company** (BN 5929) Criterion C;
- ◆ **Hans Ree Tannery** (BN 0414), Criteria A and C; and,
- ◆ **Norfolk-Southern Roundhouse** (BN 0676), Criteria A and C.

However, at this time we cannot concur with the report's findings for the **(Former) Olive General Store Building** (BN 0530) and the **Fine Arts League of Asheville** (BN 3791). We believe that the Olive General Store is not eligible for listing in the National Register under Criterion A due to the lack of interior integrity. With the removal of the second floor and the complete renovation of the interior space, the property lacks the essential physical characteristics relating to both the general store and the clubs and associations that met upstairs. The alteration of the exterior, notably the bricked-in storefronts and the modern doors and windows, has compromised the integrity needed for eligibility under Criterion C. While the Fine Arts building retains better exterior integrity, we do not believe that its architectural significance rises to the level of a property individually eligible for listing in the National Register.

For the purposes of compliance with Section 106 of the National Historic Preservation Act, we concur that, barring additional information to the contrary, the following properties are *not* eligible for listing in the National Register of Historic Places:

- ◆ **Asheville Auto Parts Buildings** (BN 5930);
- ◆ **Used Car Lot** (BN 5931);
- ◆ **(Former) Southern Coal Company Buildings** (BN 5934);
- ◆ **(Former) J. M. Westall Lumber Company Building** (BN 0339);
- ◆ **Asheville Cotton Mill/Cone Mills Office** (BN 0229/BN 5943);
- ◆ **Earle-Chesterfield Mill and Feed Company** (BN 0233);
- ◆ **Mill Workers House** (BN 5936);
- ◆ **Mill Workers House** (BN 5937);
- ◆ **Grey Eagle Tavern** (BN 5944);
- ◆ **Park Avenue Bridge** (BN 5950);
- ◆ **Commercial Structure** (BN 5945);
- ◆ **House** (BN 5946);
- ◆ **Plumbing Supply Company** (BN 3832/BN 3833)
- ◆ **Brick Warehouse** (BN 2263);
- ◆ **12 Bones Restaurant** (BN 5932);
- ◆ **The Soapy Dog** (BN 5947);
- ◆ **Parker Oil Company** (BN 5948);
- ◆ **(Former) Textile Manufacturing and Warehouse** (BN 3789);
- ◆ **J. A. Baker Packing Company** (BN 5938);
- ◆ **Nourish and Flourish Juice Bar** (BN 3784);
- ◆ **(Former) National Biscuit Company** (BN 3785);
- ◆ **(Former) Coffee Mill and Grocery** (BN 3786);
- ◆ **Condominiums** (BN 3790);
- ◆ **Asheville Greenworks** (BN 3787);
- ◆ **Glen Rock Hotel** (BN 0400);
- ◆ **Studio 375** (BN 3788);
- ◆ **Day's Tobacco Warehouse** (BN 0358);
- ◆ **Railroad Utility Building** (BN 5939);
- ◆ **Truck Repair Building** (BN 5933); and,
- ◆ **Carrier Bride** (BN 5940).

After taking into account comments from us and other parties, please transmit a letter containing the City of Asheville's final determinations of eligibility before continuing with consultation to assess what effects the undertaking may have on historic properties. When available, please forward two color hardcopies and one digital copy of the final report.

The above comments are made pursuant to Section 106 of the National Historic Preservation Act and the Advisory Council on Historic Preservation's Regulations for Compliance with Section 106 codified at 36 CFR Part 800.

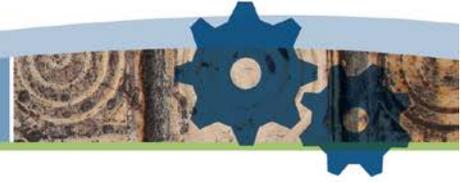
Thank you for your cooperation and consideration. If you have questions concerning the above comment, please contact Renee Gledhill-Earley, environmental review coordinator, at 919-807-6579. In all future communication concerning this project, please cite the above-referenced tracking number.

Sincerely,



for Ramona M. Bartos

cc: Dan Baechtold, City of Asheville, dbaechtold@ashevillenc.gov
Robert Ball, CDM Smith, ballrw@cdmsmith.com
Zahid Baloch, NC DOT, zbaloch@ncdot.gov
Michael Batuzich, FHWA, Michael.Batuzich@dot.gov
Mary Pope Furr, NC DOT, mfurr@ncdot.gov
Stacy Merten, Asheville & Buncombe County HRC, smerten@ashevillenc.gov
Rajit Ramkumar, CDM Smith, ramkumarr@cdmsmith.com



APPENDIX D

Public Outreach Documentation and Meeting Summaries

River Arts District Transportation Improvement Project - A section of the Wilma Dykeman Riverway

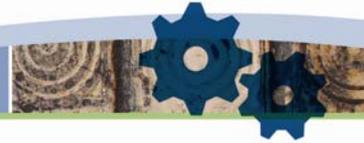
Public Involvement / Stakeholder Input Meetings / Presentations / Website Updates / Newsletter - Working List - to be Updated Through

Prepared by LandDesign

Last Updated: February 21, 2012

Name	Audience	Date	Place*	Notes / Meeting Type
Kick-off Event	Public	6/22/2010	Seven Star Factory	Speakers: Mayor Bellamy, Ken Putnam Karen Cragolin, Jane Mathews, Terry Snow, Stephanie Pankiewicz
Kick-off Meeting	Consultant	6/22/2010	LandDesign office (Grove Arcade)	Full Consultant Team + City's project manager Dan Baechtold
Rockin Riverfest 2010	Public	8/14/2010	French Broad River Park	S. Pankiewicz worked at RiverLink volunteer table to gather signatures from those interested in project updates
Primary Partners	Invited	8/18/2010	Grove Arcade Building (lunch from Clingman Café)	Consultant team attendance / presentation / open discussion
RiverLink Commitment	Invited	8/18/2010	RiverLink Offices, Study Area	Meeting at RiverLink office in a.m., study area tour in p.m. (WSA, LandDesign, SDS, Mathews attending)
City of Asheville Greenway Commission	Public	9/9/2010	City Hall (1st Floor)	Presentation to Greenway Commission (Stephanie Pankiewicz)
Asheville Area Riverfront Redev. Comm.	Public	9/9/2010	Chamber of Commerce	Presentation to AARR Commission (Stephanie Pankiewicz, Dan Baechtold, Terry Snow)
Steering Committee Mtg # 1	Invited	9/21/2010	City Hall (5th Floor)	Open discussion. City updated project description & posted to the City's website following this meeting
Study Area Tour	Invited	9/24/2010	Study Area Tour	S. Pankiewicz individual mtg w/ Janet Kagan/Percent for Art Collaborative (Internal Advisory Team Member)
MPO TAC	Public	9/26/2010	Land of Sky Regional Council	Impromptu project update by SP as invited by Jan Davis/MPO TAC Chair during public comment period
Advisory Committee Study Area Tour ^{2,3}	Invited	9/29/2010	Red Trolley Study Tour	All Steering Committee members invited as well. (WSA, LandDesign, Mathews attending)
Stakeholder Interviews (Multiple)	Invited	9/30-10/1/2010	Asheville Greenworks	Open Discussion
ASCE Fall Conference	Registrants	10/1/2010	Renaissance Hotel	Presentation / Q&A (Terry Snow & Stephanie Pankiewicz)
CIBO Issues Breakfast Mtg	Invited	10/8/2010	Biltmore Square Mall (food court)	Presentation to CIBO / Q&A (Stephanie Pankiewicz, Dan Baechtold w/ SDS & MACTEC attending)
Advisory Committee Mtg # 2	Invited	10/13/2010	Fine Arts League of the Carolinas (RAD studios)	Consultant team presentation / open discussion. 1 of 3 sit down mtgs planned w/ this group
City of Asheville Greenway Commission	Public	10/14/2010	City Hall (1st Floor)	Presentation to Greenway Commission (S. Pankiewicz)
Stakeholder Interview - Artists	Invited	10/20/2010	Fine Arts League of the Carolinas (gallery)	Open discussion. Galley is in the Grove Arcade building.
Stakeholder Interview - ADA Exec. Dir.	Invited	10/18/2010	Asheville Downtown Association office	Follow up to stakeholder invitation - ADA not able to participate previously
LORSC Brownfields Conf.	Registrants	10/27-10/28/2010	The Venue (Downtown Asheville)	Matt Wallace attended as registrant. The RAD/overall Wilma Dykeman Riverway was presented by City Staff.
Local History & Preservation Roundtable	Public	10/28/2010	Masonic Temple (Downtown Asheville)	SP attended mtg as part of public but participated when in this project became part of discussion
NCSITE Annual Mtg	Registrants	11/18/2010	Raleigh, NC	Presentation / Q&A (Terry Snow, Stephanie Pankiewicz, Rajit Ramkumar) Ken Putnam acknowledged in audience
City Manager's Development Forum	Public	11/19/2010	City Hall	Dan Baechtold presented RADTIP update
Steering Committee Mtg # 2	Invited	12/8/2010	City Hall	Open discussion. T. Snow and S. Pankiewicz attended
Stakeholder Interview - Developers	Invited	12/8/2010	Asheville Board of Realtors	Recommend by Advisory Committee member Mike Butrum
Buncombe Co. Greenway & Trails Comm.	Public	12/8/2010	County Greenway office	Presentation to Greenway Commission (Stephanie Pankiewicz, Terry Snow)
Sierra Club	Invited	1/5/2011	Unitarian Church	Presentation requested by Sierra Club / City of Asheville Greenway Commission member
Public Mtg # 1 Notice	Public	January	Electronic & Mail	Hard copies mailed to study area property owners + 100 feet offset
Public Mtg # 1 Notice	Public	January	Personal Delivery	Hand delivered to each RAD artist studio
RAD Artist Monthly Meeting	Invited	January	Odyssey Center	Monthly meeting for organization, invited by Constance Williams RAD president
NEWSLETTER	Public	January	Electronic & Hard Copy	Hard Copies distributed at Public Mtg # 1, posted to City's website & sent electronically to email list
Individual Stakeholder Interview	Invited	1/21/2011	Riverview Station	w/ Riverview Station owners (Helanie & Trudy + several tenants). Recommendation by S. Monson
Public Officials Update	Invited	1/21/2011	City Hall	City Council, Staff, County Commissioners, MPO, NCDOT, AARRC invited
Public Mtg # 1	Public	1/22/2011	AB Tech	Dan, S. Monson, Barb Mee, Terry, John M, Rajit, SP, Beth, K. Walker. 30 day comment period
City of Asheville Greenway Commission	Public	2/10/2011	City Hall (1st Floor)	Presentation to Greenway Commission (S. Pankiewicz)
Asheville Area Riverfront Redev. Comm.	Public	3/10/2011	Habitat for Humanity	Presentation to AARR Commission (Stephanie Pankiewicz)
Between the Bridges	Public	3/23 - 3/25/2011	RAD (various locations)	S. Pankiewicz, T. Snow, Meg Nealon, Beth Poovey, D. Baechtold attending various portions
Steering Committee Mtg # 3	Invited	4/6/2011	City Hall	Open discussion. T. Snow, Rajit, Michelle Peele and S. Pankiewicz attended
AB Tech Student Presentation	Public	5/6/2011	AB Tech	Terry Snow attended to represent project team / team has provided students background/base info
Asheville Area Riverfront Redev. Comm.	Public	5/12/2011	Chamber of Commerce	Presentation to AARR Commission (Stephanie Pankiewicz, Dan Baechtold, Terry Snow)
NEWSLETTER	Public	7/1/11	Electronic	Sent to email list (approx 200 emails on list of those who have attended mtgs or expressed interest)
City Staff/Tech Review Committee	Invited	7/6/2011	City Hall	
AARRC Planning Workgroup	Invited	7/6/2011	City Hall	
Advisory Committee Mtg # 3	Invited	7/6/2011	City Hall	Advisory Committee Invited to drop in at 4 p.m. for project update (several attended)
Team Worksession in Study Area	Invited	7/22/2011	Asheville Greenworks	Day Long Working Session in RADTIP w/ City Staff
EPA/FHWA/NCDOT Webinar	Invited	8/10/11	Webinar	Rajit facilitated
City SWM/State Floodway Webinar	Invited	8/25/11	Webinar	SP, Rajit, Jeff + 1 other WSA, McCray + CoA SWM staff, State NCEM floodway staff (2)
Team Worksession in Study Area	Invited	8/26/2011	Asheville Greenworks	Day Working Session in RADTIP w/ City Staff

Advisory Committee Update	Invited	8/26/2011	Asheville Greenworks	Advisory Committee Invited to drop in at 4 p.m. for project update (several attended)
Individual Stakeholder Interview	Invited	8/26/2011	Riverview Station	w/ Riverview Station owners (Helanie & Trudy + several tenants). Rajit, Stephanie, John Mettile
Project Informational Meeting with NCDOT	Invited	9/7/2011	NCDOT	Rajit facilitated, Zahid, Teresa, John, Marta attending
EPA/FHWA/NCDOT Study Area Tour & Work	Invited	9/13/11	Pink Dog Creative Studios (lunch from the Junction)	S. Pankiewicz, Rajit, John, Melissa, Jeff Mize, Dan Baechtold, Stephanie Monson attending
AARRC Worksession	Invited	9/13/11	Pink Dog Creative Studios	S. Pankiewicz, Rajit, Joh, Melissa, Jeff Mize, Dan Baechtold, Stephanie Monson attending
Stakeholder Meeting	Invited	9/13/11	Riverview Station	S. Pankiewicz, Rajit, John attending
Public Mtg # 2 Notice	Public	September	Electronic & Mail	Hard copies mailed to study area property owners + 100 feet offset
Public Mtg # 2 Notice	Public	9/20 & 9/21/2011	Personal Delivery	Hand delivered to each RAD artist studio
Steering Committee Mtg # 4	Invited	9/26/2011	Webinar	Preview of Public Mtg Presentations (City Staff & RiverLink, project team attending)
Public Officials Update	Invited	9/29/11	Habitat for Humanity Mtg Room	2 - 4 p.m. City Council, Staff, County Commissioners, MPO, NCDOT, AARRC invited
Public Mtg # 2	Public	9/29/11	Habitat for Humanity Mtg Room	4 - 7 p.m. SP, Beth, Kimberly, Rajit, Terry, Jeff, Janet Kagan, Dan Baechtold attending
NEWSLETTER	Public	9/29/2011	Hard Copy & Electronic	Distributed at Mtg & in Public Mtg material CDs provided to Public & City as follow up
East of River Kick-off	Public	10/7/11	WC Reid	S. Pankiewicz coord w/ ADC/GO/CoA staff to provide materials & exhibits for meeting
City of Asheville Bike / Ped Task Force	Public	10/10/2011	City Hall (1st Floor)	Presentation to Task Force (Dan Beachtold)
AARRC Planning Workgroup	Invited	10/11/2011	GoTo/Webinar	Worksession w/ AARRC Planning work group (Stephanie P, Rajit, Stephanie Monson attending)
City of Asheville Greenway Commission	Public	10/13/2011	City Hall (1st Floor)	Presentation to Greenway Commission (S. Pankiewicz)
Follow up Public Mtg #2 Materials	Invited	10/13 & 10/14/2011	Hand Delivery	S. Pankiewicz hand delivered Public Mtg # 2 materials to stakeholders, Advisory Committee, City Staff
Asheville HUB	Public	10/18/2011	Public Works	S. Pankiewicz coord w/ HUB/CoA staff to provide materials & exhibits for meeting
City of Asheville Greenway Commission	Invited	10/28/2011	GoTo/Webinar	Worksession w/ Greenway Commission (Stephanie & Dan attending)
CIBO Request for Input	Invited	11/1/2011	Email Blast Issued by CIBO	CIBO issued email to members requesting that they input on the current RADTIP plans (SP received a copy)
Historic Resources Commission/NCDOT/FH	Invited	11/3/2011	NCDOT	
WDR RADTIP Project update to Steering & .	Invited	1/12/2012	Electronic & Mail	
City of Asheville Parks Department 4(f) disci	Invited	2/9/2012	City Hall (5th Floor)	Rajit, Jeff, Dan, Stephanie M., Al Kopf. S. Pankiewicz, John, Rebecca on phone
FHWA/NCDOT Meeting to discuss 4(f) resoi	Invited	2/21/2012	NCDOT	Rajit, John, Dan, Donnie Brew & Mitch (FHWA), John Conforti, Zahid (NCDOT)
Steering Committee Mtg # 5	Invited	5/1/2012	City Hall (5th Floor)	Rajit, SP, Terry, Meg, Beth, John Mettile, Rebecca?, Dan Beachtold, S. Monson
Advisory Committee Updates	Invited	5/1/2012	FLS Energy (River Arts District Location)	Rajit, SP, Terry, Meg, Beth, John Mettile, Rebecca?, Dan Beachtold, S. Monson
Individual Stakeholder Interview	Invited	5/2/2012	Riverview Station	w/ Riverview Station owners (Helanie & Trudy + several tenants). Rajit, Stephanie
NCAMPO Conference	Registrants	5/3/2012	Renaissance Hotel	Rajit, SP, Dan. "Liviability and Context" Track
Congressman McHenry Study Area Tour	Invited	6/13/2012	Study Area Tour	w/ Advisory Comm member Mike Butrum. SP & Rajit prepared materials & coord. S. Monson & Dan attended
City Staff/Tech Review Committee	Invited	7/10/2010	City Hall (Public Works)	Rajit, SP, Beth, Jeff, anyone else?
Norfolk Southern RR	Invited	7/18/2012	Asheville Area Arts Council (lunch from Clingman)	Site Visit & Mtg. Rajit, Terry, Stephanie, Jeff Mize, Beth Poovey, Dan Beachtold, S. Monson + other City Staff
Consulting Parties 106 Process Meeting	Invited	9/13/2012	Asheville Area Arts Council + Webinar	Rajit, Terry, Stephanie, Beth Poovey, John, Rebecca, David Spector, Dan Beachtold
Advisory Committee/Stakeholder Drop-in (in	Invited	9/13/2012	RiverLink	Rajit, Terry, Stephanie, Beth Poovey, John, Rebecca. Helanie Greene + 1 artist/tenant attended
Asheville Area Riverfront Redevelopment Cc	Public	9/13/2012	Chamber of Commerce	Rajit, Terry, Stephanie, Beth - request to give project update due to visit by Feds (East of Riverway)



Public Meeting Summary

The first public meeting was held at AB Tech on January 22nd, 2011 to explain the project to the public and to obtain public input. The workshop was advertised on local media and also was publicized as public notice mailed to study area residents as well as business owners. The meeting notices were also hand delivered to the study area art studios. Fifty nine (59) attendees attended the first Public Meeting. A PowerPoint presentation with an overview of the project was presented in one part of the room. The slide show provided citizens with a brief overview of the projects propose and need, existing resources within the study area, alignment options, and public involvement opportunities. An overview of historic properties overview in the study area was also presented in another room. A third PowerPoint presentation included an overview of the NEPA process for the project. Maps of the existing condition, study area alternatives, precedent chanter images, and typical cross sections were presented for public review and input. The project team obtained public input from two exercises; one involved “Riverway Dollars” where citizens were give four dollars to spend on six proposed improvements. Citizens were also encouraged to suggest improvements. Another exercise involved placing a colored dot from the list of alternatives presented. Citizens were invited to speak with members of the project team and also were asked to fill out a comment sheet. Approximately four comment forms were received as well as 30 emails. An additional ten comments were made during the public workshop. A summary of the first public meeting was also posted following it on the City of Asheville’s blog. Project reports, exhibits were also posted on the city’s project initiative website.

Community Concerns and Comments

As a result of the First Public Meeting, concern and comments raised by the citizens in the community came in the form of questions, suggestions and discussion during the public workshop and comments received via email to the project team during the comment period. Below is a summary of concerns; a full list of comments is also attached.

Preference on an alternative – Several citizens voiced an opinion on their preferred alternative. The purple alternative was preferred by majority of citizens. Majority of the meeting attendees suggested that the purple alternative offered the most opportunities for revitalization in the study area. However, some recommend keeping existing alignment. Other comments included a one way pair with a road in front and a road in back of the Riverview Station building. Some comments suggested considering various hybrids of the color coded alternatives.

Business impacts – With the exception of concerns to a loading dock area, no negative impacts to business were expressed in the comments but some property owners have requested more information regarding business impacts from the alternatives.

Historic Protection – Minimize historic properties impact and protection of historic resources in this project area, including resources that are included in the Asheville Riverside Industrial Historic District.





Safety – Citizens suggested properly-sized and continuous sidewalks, marked crosswalks, and consistent and clear signage. The comments also suggested the need for better pedestrian environment, river access and separating cars and pedestrians as much as possible.

Purpose and Need - Asheville Downtown Association (ADA) supported the primary purpose and need statement of the River Arts District Transportation Project, as well as the secondary purpose statement.

Pedestrian Bridge – Public suggested the need for Bike/ped access to the river from the RiverLink bridge as well as a separate bike/ped bridge.

Typical Section – Consideration of narrower lane widths for motorists and wider bike lanes and sidewalks were suggested. A minimum of 5 foot lanes for bicycle lanes and six foot sidewalk were suggested.

Helping with the outreach – The citizens were excited about the opportunity of the proposed project and the proposed change that the project would bring to the River Arts District. Suggestions included Green Opportunities Van as an outreach initiative for the surrounding communities of Livingston Street, Hillcrest, Walton Street, etc. and the Asheville Design Center has volunteered to work with the team on this issue of outreach

Character of the area – Several comments reemphasized the overall vision of the area. The public comments support the multimodal transportation need and the 'Complete Street' design for the corridor. Implementation of greenway and safer bike/ped transportation improvements were suggested as immediate improvements.

Proximity concerns – Residents and business owners asked for information on specific impacts to their properties. As preliminary designs are developed, specific property impacts will be determined and shared with public.

Meeting Exercises

RiverWay \$ exercise

The results of the RiverWay \$ exercise are as follows:

- 14% (\$32) = Option 1 (New Bike / Ped Only Bridge - as shown on the original Riverway plan, near Riverview Station)
- 12% (\$28) = Option 2 (Upgrade Amboy Road Bridge - add bike & ped facilities)
- 11% (\$25) = Option 3 (Add Bike / Ped access ramps at RiverLink Bridge to get down to Jean Webb Park at bridge - this was a new item that came out of the Oct '10 stakeholder interviews)
- 26% (\$60) = Option 4 (Acquire land for future greenways - i.e. to complete the French Broad River Greenway - segment east)





- 13% (\$31) = Option 5 (Streetscape / Amenities - general category to cover landscape, benches, signage, etc)
- 13% (\$29) = Option 6 (Public Art)
- 7% (\$16) = Option 7 (write in category requested by several attendees "Neighborhood connection with African American - healing from past projects & more inclusion)
- 4% (\$9) = Option 8 (write category request by several attendees "Greenway at progress energy site" - i.e. this is the completion of the French Broad River Greenway segment west shown on the City's greenway's master plan)

Dots exercise

The results of the dots exercise based on 37 responses are as follows:

- Yellow Alternative – 5.4% (2 dots)
- Green Alternative – 10.8% (4 dots)
- Blue Alternative – 13.5% (5 dots)
- Cyan Alternative – 0% (0 dots)
- Purple Alternative – 70.3% (26 dots)
- Orange Alternative – 0% (0 dots)

The purple alternative was the preferred choice among the attendees. Both exercises provided snapshots of input from the public meeting. Some meeting attendees noted that the public workshop presented a lot of information to take in and they needed more time to consider a preference so they did not vote in the dots exercise.

Public Officials Meeting

City and project team representatives held a public officials meeting with local elected officials. The meeting was held on January 21st, 2011, prior to the first public meeting and seven (7) public officials attended. The purpose of the meeting was to familiarize public officials with the project and provide a schedule of events for the public workshop. The public meeting process and project exhibits were presented at the meeting.





Public Workshop #1 Notice for the River Arts District Transportation Project



The City of Asheville cordially invites you to an informational workshop for the River Arts District Transportation Project.

Purpose of the meeting is to obtain input on the following:

- **Potential corridors**
- **Environmental and community features**
- **Project purpose and need**

PROJECT DESCRIPTION and LIMITS: The purpose of the project is to improve the transportation infrastructure and the multimodal system linkages along Lyman Street and Riverside Drive between Amboy Road and Hill Street which contains the River Arts District.

The River Arts District Transportation project focuses on a 2.2 mile section of the planned Wilma Dykeman Riverway. The project is currently funded for the Environmental Documentation and the Preliminary Engineering phase. Information from this meeting will be used to refine the project purpose and need, design parameters, range of alternatives, and environmental issues.

WHEN:

Saturday, January 22, 2011
9:00 AM - 12:00 PM

WHERE:

AB Tech's Ferguson Auditorium
340 Victoria Road
Asheville, NC 28801

We look forward to your participation!

Project information regarding the completed study is available via web at:

http://ashevillenc.gov/residents/transportation/transportation_planning/default.aspx?id=23772

If you are unable to attend the Workshop, you may provide input for the study by contacting:

Stephanie Pankiewicz, RLA, ASLA
LandDesign | Asheville NC

One Page Avenue - Suite 220, Asheville NC, 28801
V: 828.281.1447 | C: 828.337.6190

Email: SPankiewicz@landdesign.com

Deadline for submissions is February 23, 2011



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Purpose and Need

The primary purpose of the project is to improve the transportation infrastructure and the multimodal system linkages along Lyman Street and Riverside Drive between Amboy Road and Hill Street which contains the River Arts District.

The secondary purpose of the project is to address the social demands to provide access to the riverfront and the revitalizing River Arts District as manifested in the various plans that culminated in the Wilma Dykeman RiverWay Master Plan and other ongoing planning activities.

Goals

- ❖ Implement previous plans (City of Asheville 2025 Plan, Wilma Dykeman RiverWay Master Plan and other City plans)
- ❖ Create a Unique Gateway for the City of Asheville
- ❖ Connect adjacent neighborhoods and downtown to each other and the French Broad River
- ❖ Promote Smart Growth, including Mixed Use and a Range of Housing Options in the River Arts District
- ❖ Increase Multi-Modal Transportation Choices – transit, bike, pedestrian and river access
- ❖ Enhance Economic Competitiveness and Sustainable Redevelopment
- ❖ Provide Recreational Opportunities for Residents and Tourists
- ❖ Improve Environmental Protection and Water Quality of the French Broad River
- ❖ Develop Opportunities for Streetscape, Amenities and Public Art



CHRONOLOGICAL LOG OF SIGNIFICANT EVENTS - PROJECT HISTORY



Project Team Kickoff - June 22, 2010

Begin Existing Conditions Investigation & Due Diligence

Study Area Tour with RiverLink – August 18, 2010

Steering Committee Meeting #1 - September 21, 2010

Advisory Committee Study Area Tour - September 29, 2010

Stakeholder Interviews – including September 30, October 1, 20 and December 8, 2010

Complete Existing Conditions Reports – November 2010

Develop Preliminary Corridors and Cross Sections – November 2010

Steering Committee Meeting #2 – December 8, 2010

Project Updates to local boards, commissions, organizations – On-going

Public Meeting # 1 – January 22, 2011



Photo Credits: RIVERLINK





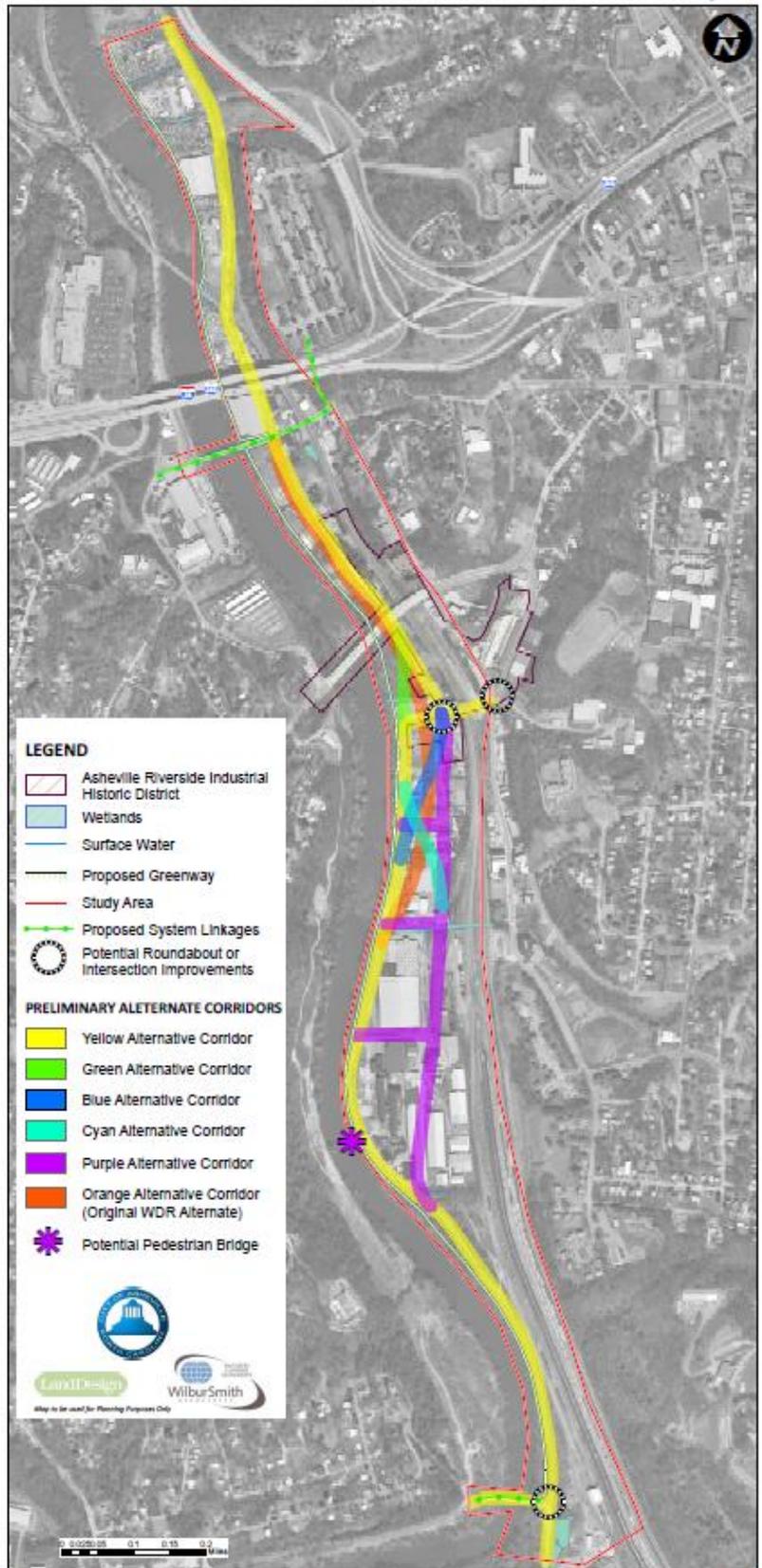
Proposed Alternative Corridors

The Project Team, which includes members of the City of Asheville, NCDOT and the steering committee has developed several concepts and potential improvements which could be incorporated into the development of the alternatives. The following alternative corridors are developed to obtain feedback :

1. Yellow Alternative Corridor
2. Green Alternative Corridor
3. Blue Alternative Corridor
4. Cyan Alternative Corridor
5. Purple Alternative Corridor
6. Orange Alternative Corridor

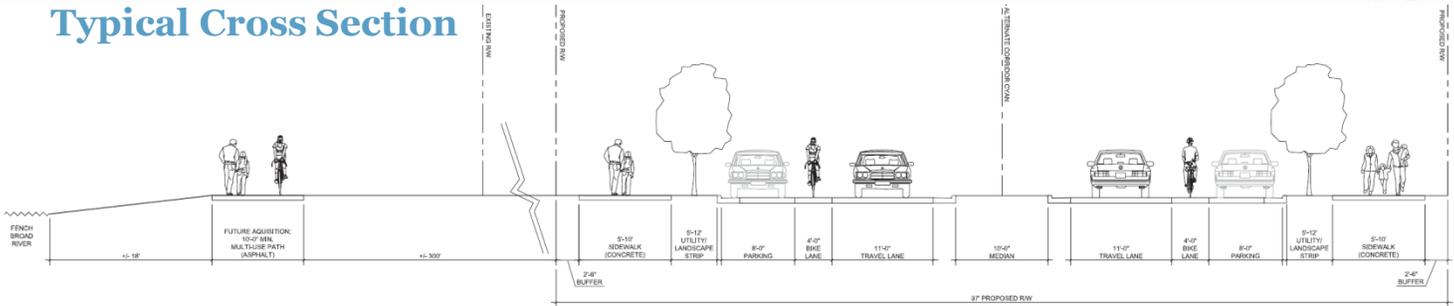
Other potential TRANSPORTATION improvements in the study area include:

- ❖ French Broad River Greenway - East segment
- ❖ Bike Lanes, Sidewalks, Transit Stops and River Access Points
- ❖ On-Street Parking
- ❖ Potential Roundabout at the Five Points intersection
- ❖ Potential Roundabout at the Amboy/Riverside intersection
- ❖ Reconfiguration of the Riverside Drive / Lyman Street intersection
- ❖ Potential Re-alignment of Lyman Street
- ❖ New Pedestrian / Bike only Bridge crossing French Broad River
- ❖ Amboy Bridge Replacement
- ❖ Bike / Pedestrian Access to/from RiverLink Bridge to French Broad River





Typical Cross Section



Public Involvement Process

1. **Primary Partners** include key agencies that will play a role in funding or implementing the plan such as NCDOT.
2. **Steering Committee** is made up of City Staff, RiverLink and NCDOT.
3. **Advisory Committee** consists of a range of civic and community leaders and advocates.
4. **Stakeholders** represent property owners, artists, businesses and community groups within or adjacent the study area
5. **Public**

Your comments are welcome

We wish to thank you for participating in today's workshop. Your comments are very important during this initial stage of the River Arts District Transportation Project. Please provide your input and feedback on :

- Potential corridors
- Environmental and community features
- Project goals and purpose and need

Please use the comment forms to make any comments or inquiries. Feel free to leave the comment forms with the project team today at the registration table or mail them by **February 23, 2011** to:

Stephanie Pankiewicz, RLA, ASLA
LandDesign | Asheville NC
One Page Avenue - Suite 220, Asheville NC, 28801
Telephone: 828.281.1447

or forward via email to:
SPankiewicz@landdesign.com



What's Next?

For project updates, please visit the City's project website at:
http://ashevillenc.gov/residents/transportation/transportation_planning/default.aspx?id=23772

Receive Comments – February 23, 2011

2nd Public Meeting – Summer 2011

Complete Study – March 2012



Photo Credits: Wilma Dykeman's family album



CHRONOLOGICAL LOG OF SIGNIFICANT EVENTS - PROJECT HISTORY



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going

Public Meeting # 1 - January 22, 2011

Public Comments Received - February 23, 2011

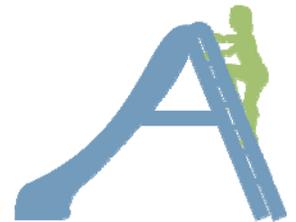
Between the Bridges Meeting - March 23-25, 2011

Steering Committee Meeting #3 - April 6, 2011

Scoping Letter Submitted to State Clearinghouse - April 15, 2011

Field Surveying and Preliminary Design - On-going

Agency Comment Period Ends - June 2011



- ! The project team is currently addressing agency comments received in June 2011
- ! Tasks for July include alternatives analysis to determine environmental impacts for the study alternatives
- ! This is a critical stage in the environmental process for the project





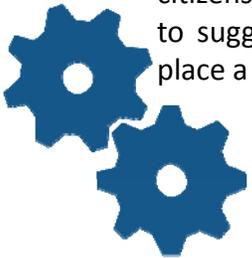
1st Public Meeting Summary

- Preference on an alternative – Purple Corridor
- Proximity / Business impacts – Proximity concerns
- Historic Protection
- Bicycle and Pedestrian Safety
- Supported for Purpose and Need
- Supported for future Pedestrian Bridge
- Wider bicycle lanes
- Helping with the outreach
- Preservation of the character of the area



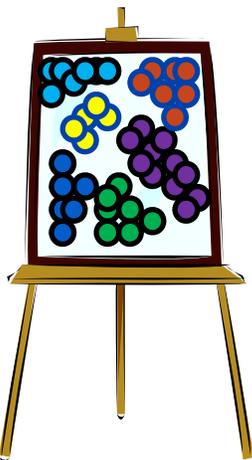
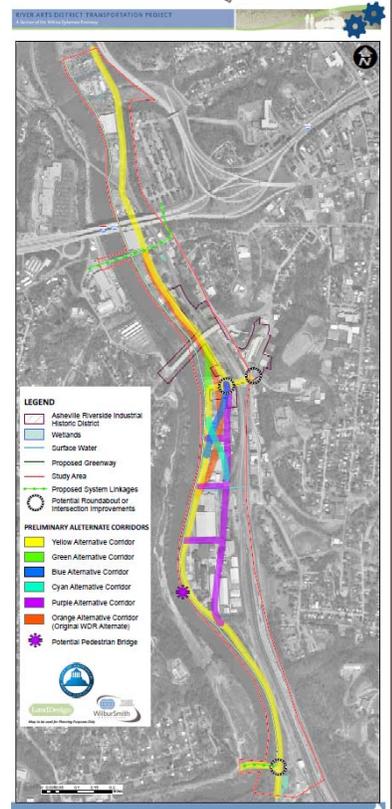
Public Meeting Exercise Results

The project team obtained public input from two exercises; one involved “Riverway Dollars” where citizens were give four dollars to spend on six proposed improvements. Citizens were also encouraged to suggest improvements. The other exercise involved “Colored Dots” where citizens were asked to place a colored dot from the list of alternatives presented.



RiverWay \$ Exercise

1. 14% = Option 1 (New Bike / Ped Only Bridge)
2. 12% = Option 2 (Upgrade Amboy Road Bridge - add bike & ped facilities)
3. 11% = Option 3 (Add Bike / Ped access ramps at RiverLink Bridge)
4. **26% = Option 4 (Acquire land for future greenways)**
5. 13% = Option 5 (Streetscape / Amenities)
6. 13% = Option 6 (Public Art)
7. 7% = Option 7 (Neighborhood connection with African American communities)
8. 4% = Option 8 (Greenway at progress energy site)



Colored Dots Exercise Results

- Yellow Alternative – 5.4%**
- Green Alternative – 10.8%**
- Blue Alternative – 13.5%**
- Cyan Alternative – 0%**
- Purple Alternative – 70.3%**
- Orange Alternative – 0%**





Proposed Roadway Characteristics



5 Foot Wide Bicycle Lanes



CONTEXT SENSITIVE DESIGN



Develop a UNIQUE Asheville Solution



Sidewalks



On-Street Parking

Preserve and Enhance Historic Character and Significance

Project's Public Involvement Process

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3. **Advisory Committee** consists of a range of civic and community leaders and advocates.
4. **Stakeholders** represent property owners, artists, businesses and community groups within or adjacent the study area
5. **Public Presentations and Input**
6. **City of Asheville Project Website**

Get Involved – Your questions, comments, and suggestions are welcome

Stephanie Pankiewicz, RLA, ASLA

LandDesign | Asheville NC

Physical Address: Grove Arcade Building
One Page Avenue - Suite 220, Asheville NC, 28801

Mailing Address:

LandDesign, PO Box 36959 Charlotte, NC 28236



PROJECT PHONE NO.: 828.450.0666

or forward via email to:

SPankiewicz@landdesign.com

Or telephone

V: 828.281.1447 | C: 828.337.6190

What's Next?

For project updates, please visit the City's project website at:

http://ashevilenc.gov/residents/transportation/transportation_planning/default.aspx?id=23772

Steering Committee Meeting No:4 - July 2011

2nd Public Meeting - August 2011

Complete Study - March 2012





Public Workshop #2 Notice for the River Arts District Transportation Project

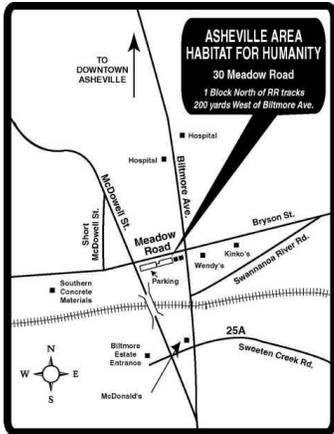


The City of Asheville cordially invites you to an informational drop-in workshop for the River Arts District Transportation Project.

The consultant team and city staff will present the study process to date and will be available to discuss and gather feedback from members of the public on the potential alternatives. This meeting is a very important step to receive community input on the preferred alignment for the River Arts District multi-modal transportation improvements.

PROJECT DESCRIPTION and LIMITS: The purpose of the project is to improve the transportation infrastructure and the multimodal system linkages along Lyman Street and Riverside Drive between Amboy Road and Hill Street which contains the River Arts District.

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WHEN: Thursday, September 29th
4:00 PM - 7:00 PM



WHERE:
Asheville Area Habitat for
Humanity's Conference Room
30 Meadow Road
Asheville, NC 28801

We look forward to your participation!

Project information regarding the completed study is available via web at:
http://ashevillenc.gov/residents/transportation/transportation_planning/default.aspx?id=23772

If you are unable to attend the Workshop, you may provide input for the study by contacting:

By Mail

Dan Baechtold, AICP

City of Asheville -Transportation Department
70 Court Plaza, Room 101
Asheville, NC 28801

By Email or Phone

Stephanie Pankiewicz, RLA, ASLA

Telephone: 828.281.1447 or 828.337.6190
Email: SPankiewicz@landdesign.com

Deadline for submissions is October 29, 2011



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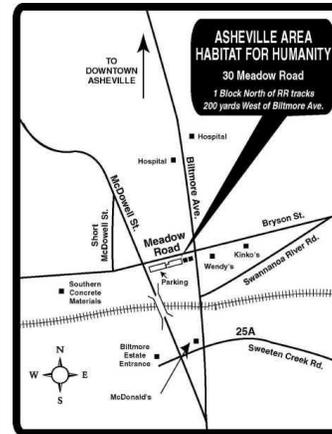


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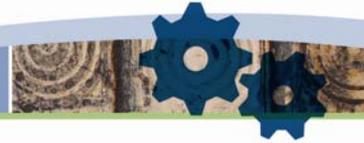
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By Email or Phone

Stephanie Pankiewicz, RLA, ASLA

Telephone: 828.281.1447 or 828.337.6190
Email: SPankiewicz@landdesign.com

Deadline for submissions is October 29, 2011



Public Workshop # 2 Summary

The second informational drop-in workshop for the River Arts District Transportation Improvement Project was held at the Asheville Area Habitat for Humanity on September 29, 2011. The project team presented study process to date and gathered feedback from members of the public on the potential alternatives. The workshop was advertised on local media and also was publicized as a public notice mailed to study area residents as well as business owners. The meeting notices were also hand delivered to the study area art studios. A total of 47 individuals attended the second Public Meeting.

Six stations were set up around the room to provide attendees with a variety of project information.

- **Station 1** presented an overview of the project's history, set up at a table containing the 2004 *Wilma Dykeman Master Plan* and other background documents and maps. This station also presented environmental features within the project area: floodways/floodplains, historic properties, hazardous materials sites, etc.
- **Station 2** displayed an interactive Google Earth 3D model, allowing attendees to explore each alternative as it relates to the existing topography, roadway network, and buildings.
- **Station 3** displayed a video of the Google Earth 3D model, flying through for each of the alternatives.
- **Station 4** presented large scale mapping of each of the alternatives with color-coded graphics to identify what elements and cross sections are included with each improvement alternative. After reviewing diagrams, attendees were invited to provide input on potential future enhancements by placing Riverway Dollars in one of six categories (separated greenway, on-street parking, landscape strip, wider sidewalk, landscape center median, and underground utilities). Feedback will be used to identify community priorities when spacing or budget limitations constrain the project.
- **Station 5** included a survey on existing deficiencies, proposed improvements, and other concerns. The draft purpose and need statement and screening matrices were also presented at this station; attendees were given blank forms to comment on these elements as well. Handouts with the alternatives were provided collect input on the preferred route of attendees.
- **Station 6** shared examples of public art to solicit feedback on how art should be incorporated within the study area.

At each station, citizens were invited to speak with members of the project team and also were asked to fill out a comment sheet. Between the meeting and following 30-day comment period, the team received alternative surveys from 25 individuals, feedback forms from 16 individuals, preferred alternative sketch maps from 22 individuals, purpose/need/goals matrices worksheets from 11 individuals, plus a petition.





Community Concerns and Comments

As a result of the second public workshop, concerns and comments raised by the citizens in the community came in the form of questions, suggestions, and discussion during the public workshop; survey and worksheets; and comments received via email to the project team during the comment period. Comments addressed a wide range of topics: alternative preferences, preferred cross-sectional elements, existing transportation needs, and features/environmental constraints to avoid. A full list of comments is attached.

Meeting Survey & Exercises

RiverWay \$ Exercise (Station 4)

This exercise was conducted to obtain feedback on the cross section priorities for the River Arts District Transportation Project. This exercise was important to determine cross section elements for the proposed facility as space is limited in some areas between the river, railroad and other historic and natural resources.

The results of the RiverWay \$ exercise is as follows:

- Undergrounding Utilities – \$67
- Separated Greenway – \$55
- Wider Concrete Sidewalk – \$38
- Large Shade Tree in Landscape Strip – \$28
- On-Street Parking – \$27
- Landscaped Median in Center – \$14

Alternatives Survey (Station 5)

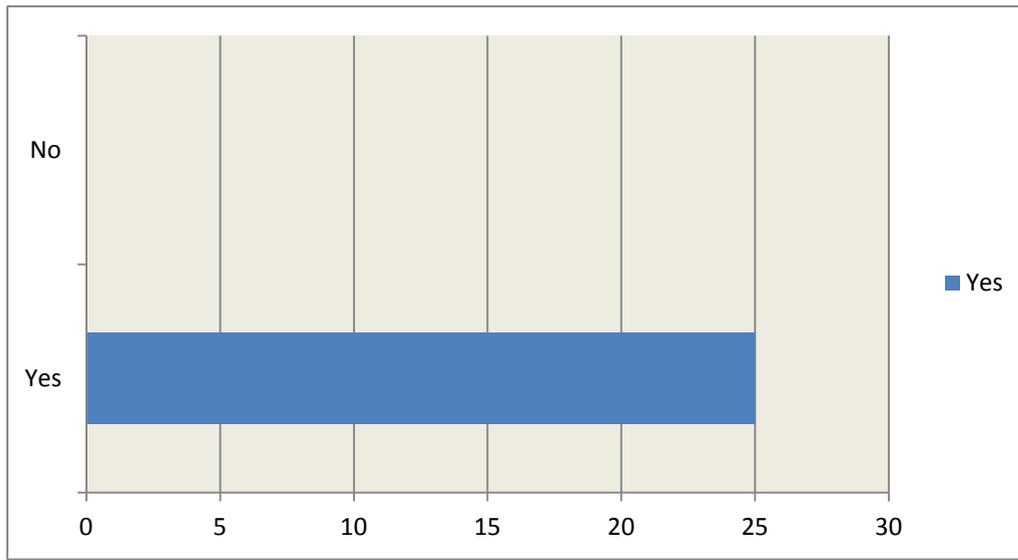
Survey forms were received from 25 individuals. Respondents unanimously agreed that Riverside Drive and/or Lyman Drive need to be improved and over 80% of respondents identified pedestrian safety as a top need. Responses indicated pedestrian safety was the top mobility need in the project area, followed by the lack of sidewalks. Several people indicated that existing safety problems will be compounded as the area develops.

Responses were well distributed when asked what types of transportation improvements were needed; sidewalks and bike lanes were the top two items identified.

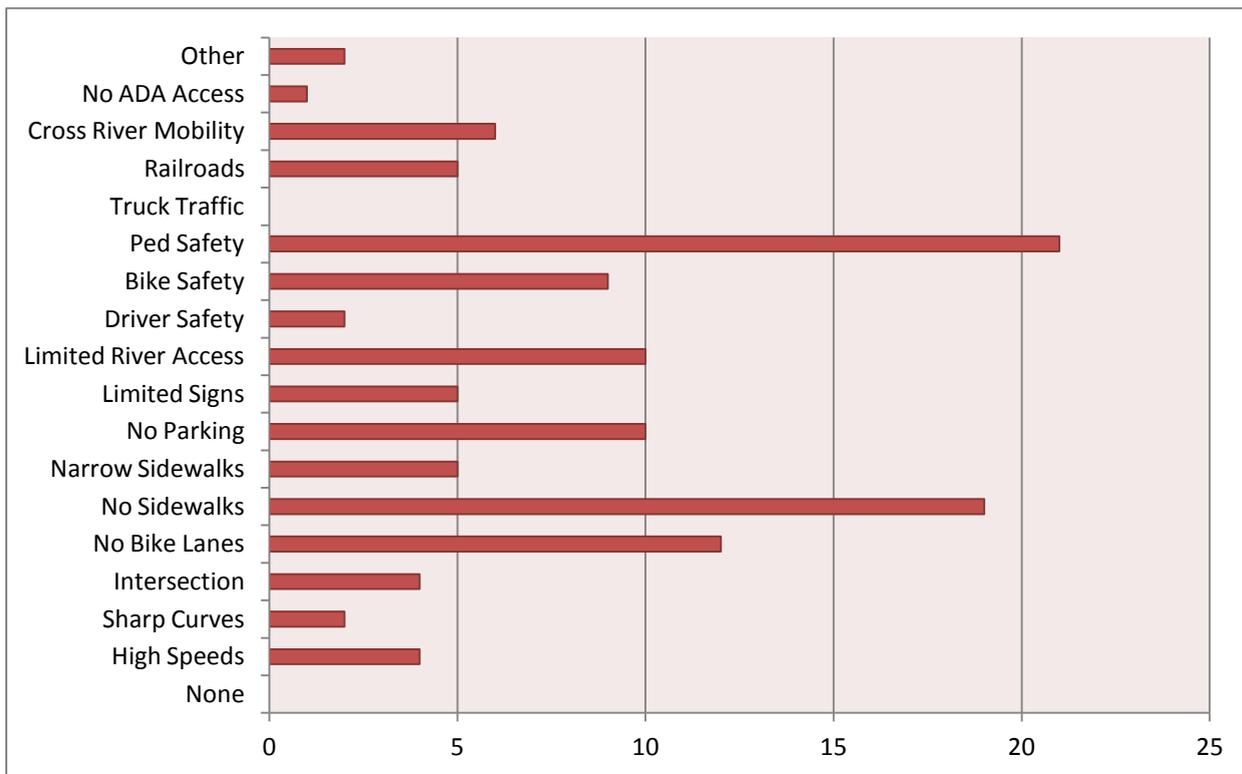
Public survey summary and public comments received during the comment period are included below.



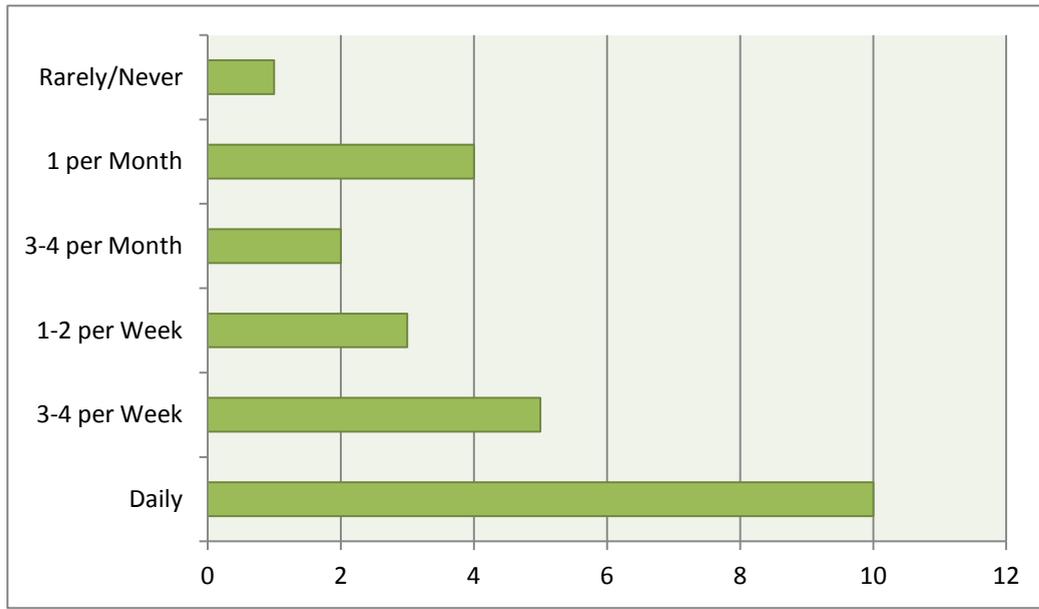
Q1. Do you think Riverside Drive and/or Lyman Drive in the River Arts District need to be improved?



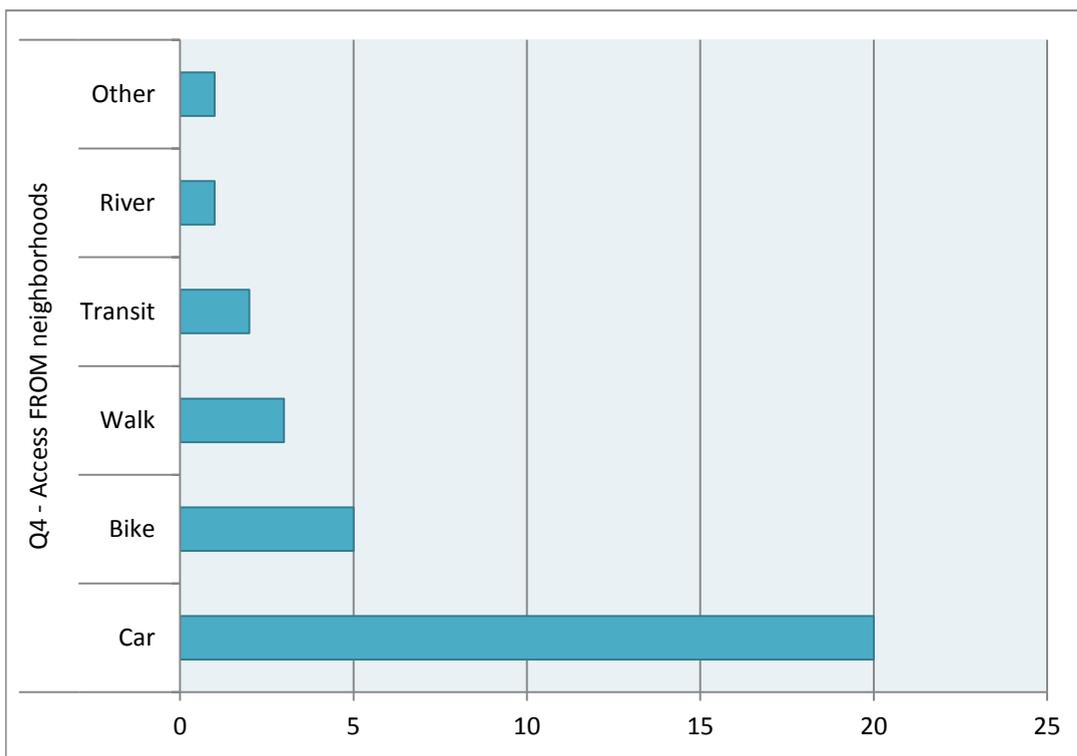
Q2. The community identified many mobility options at the January 2011 public meeting, some of which are listed below. In your opinion, what are the TOP 5 mobility problems within the River Arts District?



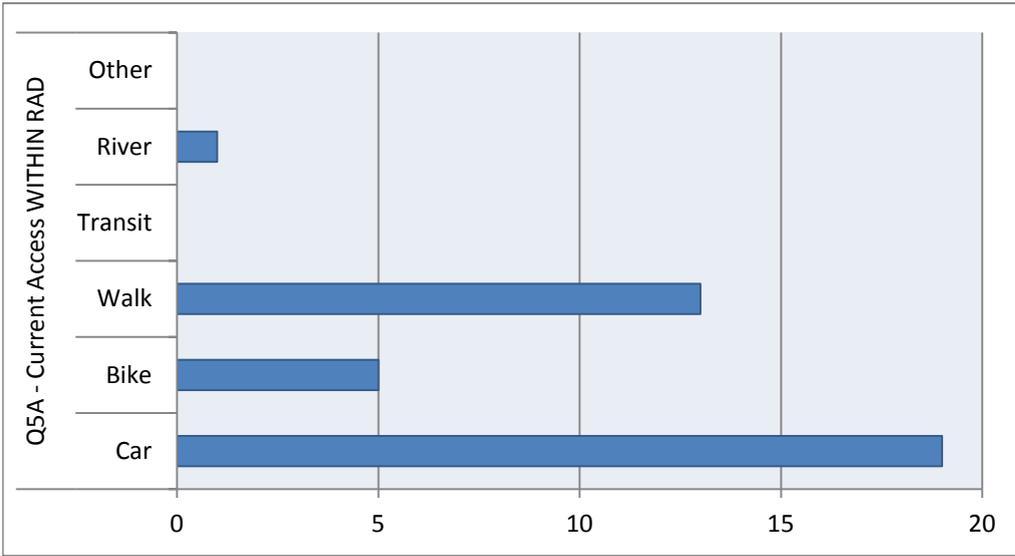
Q3. How often do you use Riverside Drive and Lyman Street in the study area now?



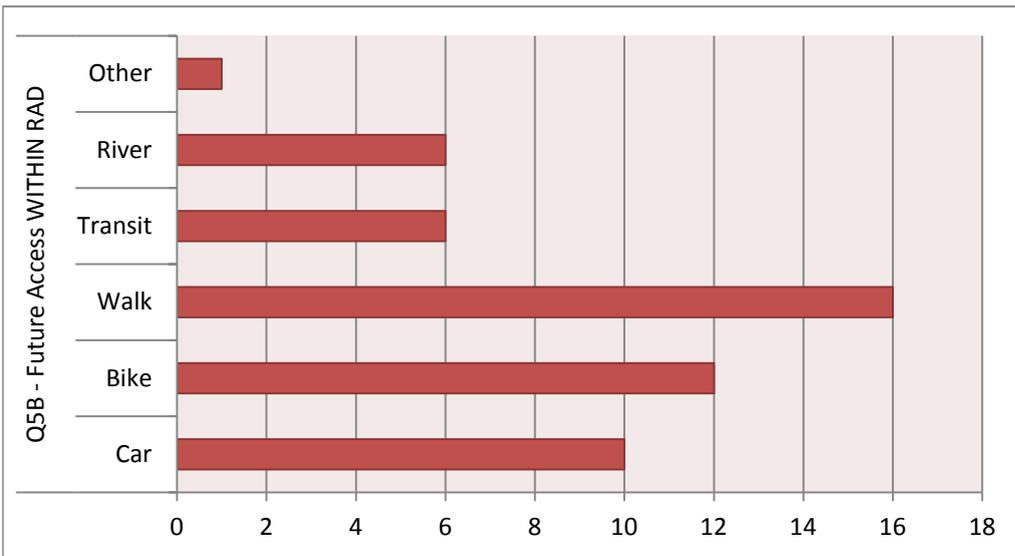
Q4. How do you currently access the River Arts District from nearby neighborhoods?



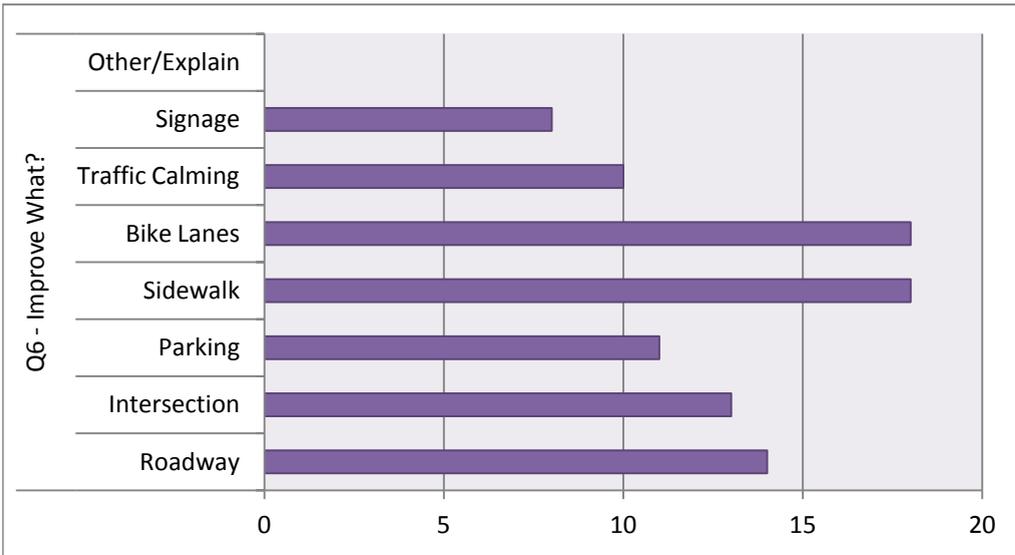
Q5A. How do you access sites within the River Arts District? (Currently)



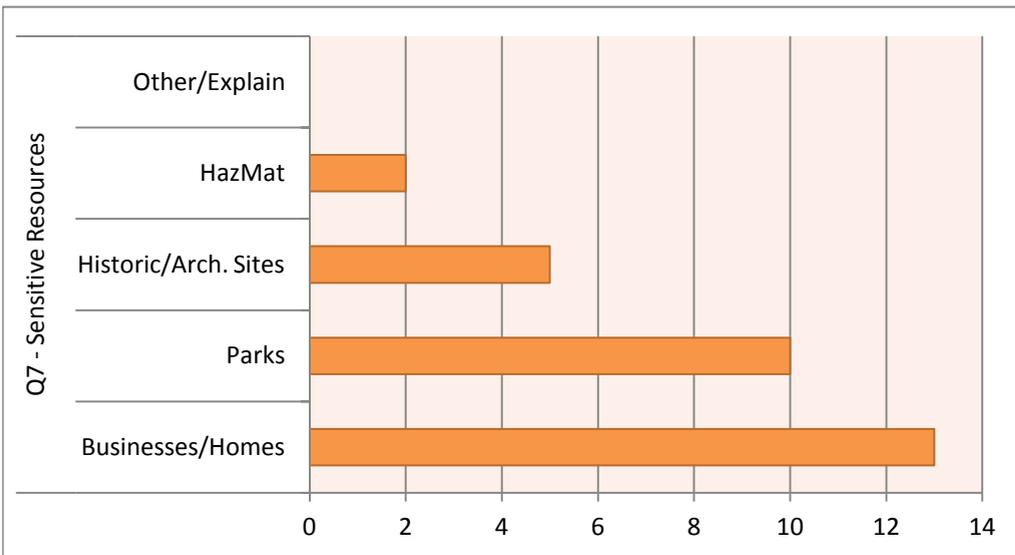
Q5B. How do you access sites within the River Arts District? (Future)



Q6. What transportation improvements should be made to Riverside Drive and Lyman Street?



Q7. Are there sensitive areas that should be considered or avoided if a new or improved route is constructed in the study area?



Public Survey Comments

Name	Organization	Q1 - Need to be improved?		Q2 - Mobility Problems		Q3 - How often?		Q6 - Improve What?		Q7 - Sensitive Resources		Q8 - Other Comments
		Yes	Explain	Explain	Explain	Explain	Explain	Explain				
Susan Rodeneck	Asheville Greenworks/Depot St	Y	Not enough amenities			Work/life in general						Instead of a planted median I'd like to consider a cable with posts in a narrow strip so there's more room for sidewalks/trees/bike lanes along sides. Median is very hard to maintain.
Brit J Oie	Riverside Studios	Y	My building is Riverside Studios and we need sidewalks and parking to help our business/visitors.	We have visitors trying to walk along Riverside Drive and it is very dangerous.		I am manager of Riverside Studios; we have six artist studios and many customers.		Riverside Drive between Curve Studios, Cotton Mill Studios, and Riverside Studios				Our immediate concern is visitors walking along Riverside Drive.
		Y	Would uplift properties along both roads	Delays because of stops		To get to my property and also to west Asheville		Eliminating stop areas.		All these alternatives split the JR Stone Sales property, decreasing its value.		
Mark Morris		Y	Vibrant need to revitalize and clean up			Commerce - through street				Ice plant - historic - needs to be preserved; rec area between bridges		Orange alternative (need for housing - ice plant is in need conversion/multi family)
Jean Webb		Y	Difficult traffic patterns make it inaccessible for tourists and congested for local traffic			Going to north Asheville from south						
Kristie Quinn		Y	Road is in need of repair.			Work-live						
Lori Meriault	Village Potters/Riverview Station	Y	Riverside desperately needs pedestrian access. Lyman improvements would enhance that entrance into the RiverArts District.	As the Arts District grows, its more populated by residents, businesses, and visitors. The more easily accessible existing places are - and safely accessible - the better for continued growth.		It's the most direct route from my house my business is on Lyman St				Riverview Station		My biggest concern is changes to Lyman Street at Section C that would negatively impact existing businesses at Riverview Station that count on Lyman Street as frontage for mobility and access.
Don Hunley	Resident/Interested Party	Y	Substandard, more traffic is coming, missed opportunity to utilize riverfront			Access businesses or offices or cut through to West Asheville		All of above				It's great that the stage can begin after the report next winter.
Stephen Jones	Advisory Committee	Y	They are unsafe			I enjoy crossing Asheville via Carrier Pk I still use Riverside to Amboy				Ecological sustainability of area		Decisions should be made for the long term - Therefore yellow and green don't work. Blue and purple seem to be the best.
Mark Bufrom	AB Realtors	Y	Development opportunities	Development opportunities. A dramatic change with public and private partnerships in an effort to transform the district		12 Bones		All of above		Private property right must be observed. Improve access and view to the river. Allow for property owners to appeal state designation (of historic structures) - make it easy. Clean up hazardous waste sites.		Maximize development opportunities - incentify developers.
Trudy Vautron Gould	Riverview Station	Y	Improvements to Lyman Street will increase appeal in River Arts District	District is growing and thriving and needs signage on the road.		Own property on Lyman Street		Narrow walks with trees - no medians		Lyman Street remains by the river in Section C		I don't like medians - hard to maintain and ugly.
Michael Sule	Asheville on Bikes	Y	Our city needs to move forward with multi-modal transportation option and our river area needs to be reclaimed and defined as an asset.	Its dangerous for our citizens to enjoy the river due to infrastructure that is outdated and built ???		To bike from Central parts of town to S and E Asheville and to enjoy F Broad Greenway		Transit. Our transportation needs to focus on multiple modes to move about the city.		Access to the river without the border of cars is a strong idea. Purple plan!		
Robert S Griffin		Y	Multimodal transportation, street trees, turn lanes, lighting, wayfinding		Street Trees	Destination & Thru traffic		Street Trees. All of above		Area between Curve Studios and 12 Bones, Riverview Station, Jean Webb Park, Rivers Edge walk, publ serv building, Mtn Energy Brownfield & aboveground storage tanks, street trees please.		Be careful. Do not assume transmission lines will be buried. Try to deal with them!
Leonard Greene	Riverview Station, 191 Lyman St	Y	It would make the area more attractive to visit	Businesses need signage on the road		To help my wife with her work at Riverview Station				Lyman Street should remain by the river in Section C		
Sarah Rolland	Village Potters/Riverview Station	Y	I believe that the improvements to Lyman will increase the appeal to the River Arts District	The River Arts District is a growing and thriving Businesses need signage options on road.		Driving six days a week to our business in Riverview Station		Sidewalks down existing Lyman with Section C remaining along the river		Lyman Street remains (by river) the same in Section C		Again we at the Village Potters feel it is critical to our business for the road to stay in front of Riverview Station
Helaine Greene	Riverview Station	Y	I believe the improvements to Lyman St will increase the appeal in the River Arts District	The district is growing and thriving. Businesses need signage on the road.		My office is in Riverview Station		Greenway		Lyman Street remains by the river in Section C		
Mark Merlin	Loren Cook Co	Y	Improvements are need to take the area from a predominantly industrial area to a mixed use community.	Several of my employees and myself walk the area during lunch and the view and safety could be much improved.		I work in the area daily.				Loren Cook Bldg, 191 Lyman Street Ste 400 & 1/2 of DayWHS @339 Lyman Street. Loren Cook employees average 15 people with a payroll of about \$450,000/year supporting \$6.5M in sales for the company. We lease mfg space from Riverview Station and warehouse space in the Day's Warehouse from Asheville Waste Paper.		Loren Cook requires both manufacturing and warehouse space to continue operations. We have been part of the district for over 20 years.
		Y	It is a mixed bag today and needs to get a design overhaul.			short cut.						
Darren Green		Y				travel to/from my business				Loading dock at 99 Riverside Drive		
Susan Weatherford		Y	Need easier flow of traffic (cars, bikes, peds)	We need to support more biking & walking access to the river. More parking will just bring too many cars.		To go to art studios; to go to Phil Mechanic bldg for writing events; to ride my bike.						
Robin Cape		Y	An upgraded road will encourage positive redevelopment			Avoid downtown traffic						

Patty Torno	River Arts District/WECAN	Y	A lot of car & ped traffic ... no sidewalks ... no bike lanes on Riverside ... poorly designed intersection at Curve		I live/work at 6 Riverside Drive		To me, the new roadway should be what is best for the whole ... we will all have to give up something	
Jim Grode		Y	To improve the aesthetic and recreational aspects of the area, thereby spurring context sensitive redevelopment	The roads are currently designed only for cars, not for people, and therefore do nothing to encourage people to come to, stay in, or move within the area				
C. McIntyre		Y	Alot of effort has been put into revitalizing this area; projects like this are a great way to work with business owners and attract new business to the area. It's also a good way to promote the idea of Asheville's emphasis on providing its residents with a "better quality of life"		Visit businesses			
Robbie Sweetser	Blue Ridge Bicycle Club; Griffin Architects	Y	to better enhance and update infrastructure in the area encouraging new development; to better connect public access and pedestrian/bicycle connections to the river		bicycle commuting to north Asheville to Biltmore Village; access to Amboy Rd parks			

Public Comments Received

Name	Organization/Neighborhood	Comments
Dwayne Stutzman	BC Greenwood Community	Sidewalks and on-street parking on west side is redundant. Can make bike lanes wider if not busy and then pedestrians would not need to cross from west side.
		Need 2nd Pedestrian/Bike Access from Hillcrest to Monford - Single access via existing pedestrian bridge requires Hillcrest resident to cross back over I-240 at some point to get to North Asheville.
Mike Morris	Broker	Plan Preference is "Orange" - I have a developer considering a renovation project for the Ice Plant (Historic). This includes the 1 acre and building plus 2 acres across street. Less impact to Ice Plant.
Anne Simmons		<u>Orange Plan Desired.</u> Owns Ice Plant. Developer is considering the project for housing. <u>Prefer Orange!</u>
Cynthia Thornton	Realtor - Keller Williams	I like Robert Form Plan. Add floating docks for vendors/cultural events.
Sarah Rolland	The Village Potters at Riverview Station	Please find a way to keep the existing road to the front of Riverview Station (Section C). We have a retail gallery and the road frontage is critical to our business. In your "Project Purpose and Need" you state that will provide greater access to....." existing artists studios." In order to be true to this objective it is critical that Lyman Street is maintained in front of Riverview Station. The Blue Plan does accomplish this goal for us. Purple and Cyan is devastating to our business! Thank you for your consideration!!
Marty Block	Cotton Mill Studios	Keep the road off the floodway fissure there are breaks in the medians so cars can make left turns into buildings on Riverside Drive. Riverside Drive is desperately in need of a pathway (NOW) between 12 Bones and the Craven Street Bridge.
Claudia Nix		Amboy Bridge needs to be wider with bike facilities to solve traffic problems but Amboy should remain 2 lane with slower speed limit. Also don't like parking on Riverside because it will add pedestrian conflicts with motorists and bikes as people cross road. 5' bike lane with parking is too narrow, i.e. of opening doors if continue with parking - bike lane - it needs to be 6'. But maybe taking space from the median would help.
Constance Williams	RAD	Round-about at 12 Bones Area will back up due to trains that sit on tracks for 20+ minutes. Like Yellow-Orange.
Jonathan Holden		Green, public, open space should be preserved. If a "pedestrian oriented" area is to be created, then larger sidewalks are needed in addition to pedestrian-friendly streets. This means shorter/smaller cross-sections and slower traffic speeds. Therefore on-street parking should be implemented along with curb kick-outs at crossings.
Don Hunley	County Resident - Work in West Asheville, Professional and Personal Interest	Lots of options; fun; good to have it in one room but maybe arranged a little different. Some maps are a little daunting because they are so big. Recommend more middle sized maps. Use of dots for "sensitive areas" was confusing since last time we voted for route alternatives using colored dots. Good public input process and methods.
Lori Theriault	The Village Potters at Riverview Station	As an artists with a three-fold investment in my space at Riverview Station, I hope to see a final plan that keeps Lyman Street where it is, which would preserve much needed visibility and access to our business. The Blue Plan seems to accomplish most of your stated project purpose and need, and preserves our much needed road frontage. Thank you for the process and your consideration of many opinions.
Susan Roderick	Asheville Greenworks/Depot SC	Hug the river most of the way with attractive tree lined boulevard with some green space at shore (like Merriman along _____)
Michael J. Sule	Asheville on Bikes	Purple is my choice because it routes traffic away from the river and therefore re-opens the river to people. People have direct access to river without the burden of traffic. I'm excited about this option. Greenway should be expanded to include Patten Avenue sidewalks. This infrastructure connect downtown to RAD and Central. Purple Plan! I love how this moves traffic away from the river so that pedestrians and bikes have direct access to the river.
Sarah Rolland	The Village Potters at Riverview Station	Only addressing Section "C" with - black sharpie. Keeping main traffic on Lyman in front of Riverview Station. We would support an alternative secondary road red sharpie to accommodate potential growth behind without compromising existing vital businesses like the Village Potters, 310 Art, Sassafrass Gallery, Mountain Restoration and Loren Cook and many others!
Sarah Rolland	The Village Potters at Riverview Station	This has a suggestion for Section "C" where the road would follow the path of many of the plans but brings the road around through a dilapidated non-historic building to maintain the road in front of Riverview Station protecting the "existing artist studios" as well as Loren Cook a major contributing industry in the River Arts District.

Robert S. Griffin, AIA	AARRC, Bilt Village, Preserve. Society	<ol style="list-style-type: none"> 1. Meander the road and deliberately slow down traffic also create different experience with waters edge. 2. Sacrifice medians for street trees, bulb out road for turn lanes (show turn lanes ASAP). 3. Create opportunities for development adjacent to River and Greenway. Orient roadway so that parking is not required between development and river or greenway (A) not (B). 4. Recognize fragile areas where a local street or plaza would be smarter than Riverside Drive. C 5. Do not take private property from Historic Sites that are prime for development offset road and narrow greenway.
Trudy Vautrin Gould	Co-Owner Riverview Station	Only Addressing Section C - Keep main traffic corridor in front of Riverview Station. I would support a alternative secondary road in blue dotted sharpie to accommodate potential growth behind without compromising existing artists and businesses.
Dan Hunley	County Resident - and Interested Party (professionally and personally) Work in West Asheville	I Like Orange.
Jean Webb	Riverlink	Graphic representation of preferred alternative
Mark Morris		Orange Plan Preferred. I am a broker; have a developer looking at the Ice Plant for housing.
Leanard Green	Riverview Station - 191 Lyman Street	Addressing Section C - Keep main traffic in front of Riverview Station Building (Black Sharpie). I would support a secondary road indicated with the red sharpie to accommodate potential growth behind Riverview Station without compromising existing vital businesses like the Village Potters Loren Cook, Sassafrass and Mountain Restoration and The Antiques of Riverview Station and many others.
Helaine Greene	Co-Owner Riverview Station	Only Addressing Section C - Keeping main traffic corridor in front of Riverview Station (Black Sharpie). I would support a secondary road indicated with the red sharpie to accommodate potential growth behind Riverview Station without compromising existing vital businesses like the Village Potters, Loren Cook, Sassafrass and Mountain Restoration and the Antiques at Riverview Station and many others. I would also like to see the road curve around the storage warehouses to also create a space for development.
Lori Theriault	The Village Potters at Riverview Station (Section C)	Section C, I would like to see a rate plan that maintains Lyman Street as the main route in front of Riverview Station (black sharpie), without compromising Loren Cook, and still offering a secondary access road to the proposed purple route (red sharpie).
Lori Theriault	The Village Potters at Riverview Station	Maintain Lyman (Riverview Station) in Section C, but add an access (secondary) road to give access to potential areas of development without compromising the existing businesses that currently front Lyman Street.
Stephen Jones	Advisory Committee - Hub Alliance	Make long-range sustainable alternate - Blue, Purple (cyan?)
Dwayne Stutzman		Graphic representation of preferred alternative
Mark Merlin	Loren Cook Co.	Graphic representation of preferred alternative
Marty Black	Cotton Mill Studios	Need a path (mulch?) between 12 Bones and Craven Street Bridge so tourists can safely negotiate Riverside Drive by foot.
Robert S. Griffin, AIA	AARRC	Meander road slow traffic down diversify relationship to water. If developments on the river area desirable thing create opportunities that do not require parking between development and the river (A) not (B). Try to create villages (C). Do not take away property from old Cotton Mill Site (D).
Jonathan Holden		Graphic representation of preferred alternative
Peter Mallett	Owner, JR Stone Property	Would not want any of the three alternatives.
		1/2 of Days Tobacco Warehouse Not Highlight structures impacted, corridor specific No medians!
		<p>Making a connection from Hillcrest to the River.</p> <p>Do not like green option.</p> <p>--Creates island - unsafe "BBQ Island".</p> <p>Pull Road away from river.</p> <p>Open space along river.</p> <p>No increase in traffic speed.</p> <p>Like purple alternative.</p>



Public Meeting #2

Asheville Area Habitat for Humanity
30 Meadow Road, Asheville, NC 28801
Thursday, September 29, 2011
4:00 p.m. to 7:00 p.m.

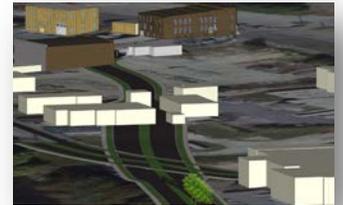
The second public meeting for this project is a highly interactive, open-house style community meeting. The consultant team and city staff will present the study process to date and will be available to discuss and gather feedback from members of the public on the current alignments of potential alternatives. This meeting is a very important step to receive community input on the preferred alternative for the River Arts District Multi-Modal Transportation Improvements. Community Input opportunities will include over six (6) Stations at which you can:



Review History of the Project & Wilma Dykeman RiverWay Master Plan (2004)



Interact with a 3D model of the all the Alternatives



View a “fly-thru” video of the alternatives



View Updated Plans, Cross Sections and other Details of the Alternatives and Complete an Exercise to Prioritize the Types, Sizes and Locations of Multi-Modal Transportation Improvements



Comment on the Draft Purpose and Need & Goals Screening Matrix; Fill out a “Mobility” and Impacts Survey Questionnaire; Review the project schedule, including process to narrow down the current alternatives to One (1) final alignment AND Identify individual preferred corridor alignment.



Visit A Public Art Visual Preference Image Station





CHRONOLOGICAL LOG OF SIGNIFICANT EVENTS - PROJECT HISTORY

Project Team Kickoff - June 22, 2010

Begin Existing Conditions Investigation & Due Diligence

Study Area Tour with RiverLink - August 18, 2010

Steering Committee Meeting #1 - September 21, 2010

Advisory Committee Study Area Tour - September 29, 2010

Stakeholder Interviews - including September 30, October 1, 20 and December 8, 2010

Advisory Committee Meeting - October 13, 2010

Complete Existing Conditions Reports - November 2010

Develop Preliminary Corridors and Cross Sections - November 2010

Steering Committee Meeting #2 - December 8, 2010

Project Updates to local boards, commissions, organizations - Ongoing

Public Meeting # 1 - January 22, 2011

Public Comments Received - February 23, 2011

Between the Bridges Meeting - March 23-25, 2011

Steering Committee Meeting #3 - April 6, 2011

Scoping Letter Submitted to State Clearinghouse - April 15, 2011

Field Surveying and Preliminary Design - Completed July, 2011

Agency Comment Period Ends - June 2011

Advisory Committee Updates (June, July and August)

Newsletter #2 Released - July, 2011

Stakeholder Meetings (such as property owner, AARRC, etc)

Meeting with City's Technical Review Committee - July 6, 2011

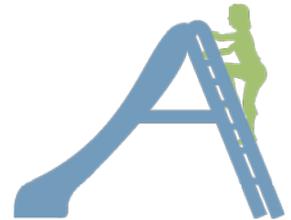
Meeting with City's Storm water Management, NCDENR, and Floodplain Mapping - August 25, 2011

EPA/FHWA/FTA Webinar - August 10, 2011

EPA, FTA, NCDOT, FHWA, City Staff Joint work session at Pink Dog Creative studio - September 13, 2011

Steering Committee Webinar - September 23, 2011

Public Meeting # 2 September 29, 2011





Public Workshop #3 Notice for the River Arts District Transportation Project



Buncombe County

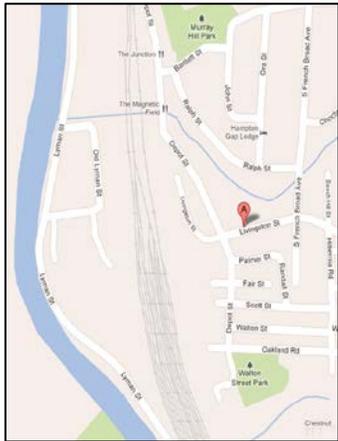
TIP: U-5019

The City of Asheville cordially invites you to the final informational workshop for the River Arts District Transportation Project on Thursday, December 6th from 4:00 - 7:00 PM. This meeting is informal, and citizens are welcome to drop in at any time.

The consultant team and city staff will present the study process to date and will be available to discuss and gather feedback from members of the public on the Recommended Alternative. The proposed improvements and their impacts will be evaluated in a Categorical Exclusion (CE) Report which is expected to be complete by December 31, 2012.

PROJECT DESCRIPTION: The purpose of the project is to improve a 2.2-mile section of Riverside Drive and Lyman Street in the River Arts District. The proposed improvements include wider roadways to accommodate on-street parking, curb and gutter, sidewalks and bicycle lanes for the traveling public and visitors to the River Arts District. Additional right-of-way acquisition and the relocation of businesses will be required to construct the project. A riverfront greenway is also proposed along the corridor.

The City will provide auxiliary aids and services under the Americans with Disabilities Act for disabled persons who want to participate in these workshops. Anyone requiring special services should contact Dan Baechtold as early as possible so that arrangements can be made.



WHEN:
Thursday, December 6th
4:00 PM - 7:00 PM
WHERE:
Dr. Wesley Grant Sr. Southside Center
285 Livingston Center
Asheville, NC 28801

We look forward to your participation!

Project information regarding the study is available via web at:

<http://www.ashevillenc.gov/Departments/Transportation/RADTransportationProject.aspx>

If you are unable to attend the Workshop, you may provide input for the study by contacting:

Dan Baechtold, AICP

City of Asheville -Transportation Department
70 Court Plaza, Room 101
Asheville, NC 28801



Tel: (828)259-5842 Email: dbaechtold@ashevillenc.gov

Deadline for comments is December 21, 2012



Public Workshop #3 Notice for the River Arts District Transportation Project



Buncombe County

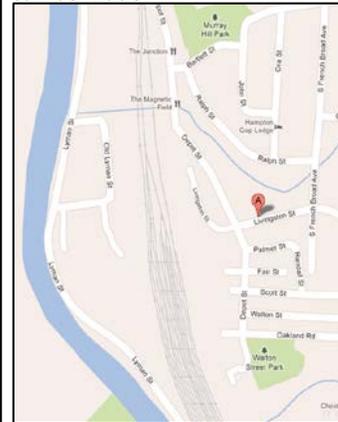
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Deadline for comments is December 21, 2012



Public Workshop Summary

A public workshop was held at the Dr. Wesley Grant Southside Community Center on December 6, 2012 to explain the project to the public and to obtain public input. The workshop was advertised on local media and also was publicized as public notice delivered to study area residents and business owners. The meeting notices were also hand delivered to the study area art studios. An estimated 98 people attended the meeting, including members of the project team.

Six stations were set up in the lobby and auditorium.

1. A PowerPoint presentation with an overview of the project was presented in the lobby. The slide show provided citizens with a brief overview of the project's propose and need, existing resources within the study area, alternatives considered, key impacts.
2. A table provided a review of the project history, including the 2004 Wilma Dykeman RiverWay Master Plan.
3. A timeline for the project outlined key meetings with stakeholders and advisory groups that occurred throughout the project.
4. Large scale illustrative plans of the Recommended Alternative were prepared to address specific concerns of stakeholders, particularly regarding elements of the Recommended Alternative and impacts to individual properties.
5. A variety of cross-section views along the Recommended Alternative were displayed.
6. A public workshop map was prepared according to NCDOT standards. This large scale map also facilitated discussions between attendees and members of the project team, particularly regarding impacts to individual properties.

Community Concerns and Comments

Concern and comments raised by participants came in the form of questions, suggestions, and discussion during the public workshop and comments received via email to the project team during the comment period. An overview of comments received (below) is presented topically.

Support for the Recommended Alternative: a number of organizations, local businesses, and citizens expressed their support for the Recommended Alternative.

- The City of Asheville supports the preferred alignment and preliminary design for the RADTIP and supports ongoing efforts to develop partnerships and identify funding sources in order to implement the project.
- The Asheville Area Riverfront Redevelopment Commission supports the preferred route for realignment of Riverside Drive and Lyman Street as shown by the RAD TIP Project team at their December 6, 2012 public meeting and as adopted by the Asheville City Council on December 11, 2012.
- Were Wilma Dykeman alive today, she would applaud the DOT's plan. She believed that the French Broad is not only a natural resource which should be preserved and enjoyed for its own





sake, but also that the river is a potential economic engine for the city, county, and region. A fully implemented RiverWay along the sections in question, as designed by the DOT, would lay the foundation for realizing much of that potential.

- The Wilma Dykeman RiverWay study final design is in line with local, state, and regional policies which encourage complete streets and a multimodal transportation system. This project will be in line with the newly adopted NCDOT Healthy Transportation policy statement.
- The plan looks great and I agree with the Recommended Alternative. I can't wait to see it in action!
- As a member of the Community Advisory Committee over the past two years, I commend the project team for their sustained, transparent, and inclusive community involvement. I can honestly say that all possible stakeholders were made aware of this process, were invited to join in, and were heard in their concerns, with those concerns being taken into account.

Concerns about 12 Bones & other businesses: the most common concerns expressed related to the impacts on RAD businesses, specifically 12 Bones.

- We should be sensitive to the four businesses impacted by the DOT's plan. I hope that proper recompense or substitution can be agreed upon going forward in order to allow the project to proceed and succeed.
- We have concerns about 12 Bones. If the business stays in its present location, the alternative would eliminate roughly 30 parking spaces that are not create close enough to be usable. Trains stop traffic today; what happens when you add a roundabout on either side of the tracks? Relocating the patio area will disrupt business; permitting within the floodplain has been an issue in the past. Income loss and business disruption will be associated with construction – the restaurant is only open when road crews work (11 am to 4 pm). If the business relocates, who pays for the relocation? How is the expense estimated? Does it include loss of income due to time spent on relocation, start to finish? Will there be issues with permitting? How is a new location selected?
- 12 Bones has hosted the President twice, arguably making it more important than the falling down, graffiti covered, brick shells of the stone yard. The curve location should be reconsidered. If the old icehouse can be demolished, why not these "historic buildings" too?
- 12 Bones opened in December 2005, before a lot of people thought that the River District was cool. How come we get thrown under the bus? And what if the city relocates it to a different part of town because they couldn't find a suitable location by the river. Then no one will give directions to the RAD using the 12 Bones location as a reference point anymore.
- My only concern is the proposed sidewalk on the east side of the road. The loading dock in front of 99 Riverside Drive is crucial to operations. It must remain clear to trucks where to pull in and out of the building.
- Make a point to hire local designers, landscapers, and plant suppliers for the contract work. We have so many talented up and coming businesses in Asheville that need continued support; the Wilma Dykeman project could be a significant boost to these local businesses.





Suggestions for Moving Forward: other responses identified suggestions to provide additional parking, improve amenities for pedestrians and cyclists, facilitate truck movements, adjust the alignment, reconfigure sidewalks, etc.

- Additional Parking
 - o As much on-street parking as possible should be provided.
 - o Increased opportunities for on street parking along Riverside Drive, specifically from the Craven intersection to the intersection of Lyman Street (at proposed roundabout) should be explored to support the vibrancy of businesses and future access to the riverfront.
 - o Consider an innovative parking solution that is successfully demonstrated in other communities we have visited: back-in angled parking. The design would be ideal in tight spaces like the area just north of the proposed roundabout and adjacent to what is currently the 12 Bones Restaurant. The design also addresses public safety as it minimizes potential conflict points between bicyclists, pedestrians, and automobiles.
- Amenities for Pedestrians & Cyclists
 - o Eliminate sidewalks on the east side of the road between Amboy and the first privately owned non-railroad parcel on the east side of Lyman and between the railroad trestle and Hillcrest. Add safe pedestrian crossings at either end of these segments to increase pervious area; walking on the river side is a more pleasant, preferable experience.
 - o The team should address connectivity to West Asheville and to the RiverLink park/greenway system. Please help West Asheville access the RiverWay and all the RiverLink parks safely.
 - o It is not clear if crossing Riverside Drive from the multiuse path traveling north beyond the railroad bridge will be safe.
 - o Consider an unpaved walking path between the Craven Street bridge and 12 Bones (along Lyman) as a temporary pedestrian solution. It is very much needed, judging by river concerts this summer.
 - o Bike lanes should be maintained, removing debris from the roadway.
 - o Consider a pedestrian bridge under the I-240 bridges, similar to the James River at the parkway.
 - o In historic/business/urban districts, on-street parking should be emphasized as much as possible. The purpose of on-street bicycle lanes is for community bicyclists (as opposed to recreational cyclists) to feel comfortable on the road. While in areas not conducive to redevelopment (i.e. industrial, floodplain areas to the north) might be best served by bicycle lanes and do not require on-street parking, the more dense and developed areas – with on-street parking – will calm auto traffic and create the desired effect. This reduces the need for inefficient and wasteful, value-reducing off-street parking. In short, I think that as much attention as possible should be paid to providing on-street parking in lieu of medians. The areas surrounding and directly east of 12 Bones are a



prime example of the location in which bicyclists, pedestrians, businesses, and motorists would be better served by on-street parking with a sharrow than a bicycle lane.

- Concerns about Truck Movements
 - o Verify that all conditions are designed for use by large trucks, often used by industrial businesses along the river.
 - o It seems absolutely vital that replacing the span of the Norfolk Southern trestle bridge over Riverside Drive and widening the Riverside-Craven intersection be included with the project. This would minimize the impact of truck traffic along the RiverWay, particularly New Belgium Brewery traffic.
 - o How will the traffic from New Belgium affect the RADTIP project? Please make sure this route will support trucks coming and going from New Belgium so they don't have to go through residential areas along Haywood Road. Build in proper turning radii for Craven and Riverside.
- Landscaping
 - o I would like to see the installation of native plants (specifically grasses) on the banks and roadsides throughout the RAD greenways.
 - o Who is going to maintain the trees and plantings?
 - o Create romantic ruins.
- Other issues
 - o Consider shifting the road slightly to the west in front of the historic Cotton Mill remnants to provide a more attractive development site and by removing the center median proposed for the area between the RiverLink Bridge and Lyman Street to eliminate the need for grounds maintenance, to reduce the overall width of the public right-of-way, to enhance access to adjacent properties, and to support pedestrian needs through a well designed crosswalk at the roundabout.
 - o Newly aligned streets may require street naming or re-naming schemes for e911 address assignment purposes. A preferred proposed street naming proposal could be initiated by the adjoining property owners prior to submitting plans for final review.
 - o Consider one way pairs on Roberts and Depot Streets.
 - o Can the project team work with Norfolk Southern to create a crossing over the rail line at Riverview Station or to connect Depot Street through to AB Tech where there is a bridge?

Public Comments Received

Name	Organization/Neighborhood	Comments
Mike Butram	Asheville Board of Realtors	As long as an equitable arrangement is worked out with 12 Bones owners, and any other established successful business, I believe the Alternative Route depicted in #6 is the best choice. I commend the committee for their diligence and fair minded approach to get to the point. Well done.
Luke W. Perry		Please make sure this route will support trucks coming and going from New Belgium so they don't have to go on Haywood Road. Build in proper turning radiuses as Craven and Riverside. Otherwise, looks pretty good.
City Council	The City of Asheville	The City of Asheville supports the preferred alignment and preliminary design for the RADTIP and supports ongoing efforts to develop partnerships and identify funding sources in order to implement the project.
	Asheville Area Riverfront Redevelopment Commission	<p>The AARRC supports the preferred route for realignment of Riverside Drive and Lyman Street as shown by the River Arts District Transportation Improvement Project team at their December 6, 2012, public meeting and as adopted by Asheville City Council on December 11, 2012. Recommendation to Asheville City Council, Project Team, and Partners: In addition to supporting the preferred alignment as shown, the Riverfront Commission affirms its intention to promote that high quality design details continue to be explored during upcoming phases of the project:</p> <p>A. Inclusion of additional dispersed parking opportunities: 1- Increase opportunities for on street parking along Riverside Drive, specifically from the Craven intersection to the intersection of Lyman Street (at proposed roundabout), to support the vibrancy of businesses and future access to the riverfront. 2- Consider an innovative parking solution that is successfully demonstrated in other communities we have visited- back-in angled parking. This design would be ideal in tight spaces like the area just north of the proposed roundabout and adjacent to what is currently the 12 Bones Restaurant. The design also addresses public safety, as it minimizes potential conflicts between bicyclists, pedestrians, and automobiles. B. Consider opportunities to create or maintain land that contributes to tax base. The creation of over two miles of greenway, open space, and 5 acres parkland provides an incredible city amenity and a corresponding need for increased city revenue that supports its creation and maintenance. Public sector fiscal sustainability can be promoted through thoughtful design. Two examples of how to create additional value for a taxable private parcel and/or reduce the burden created by non-taxable property are: 1- Shifting the road slightly to the west in front of the historic Cotton Mill remnants (and sufficiently far enough south on Riverside Drive to allow for Craven Bridge turning radius alignment) provides a future developer with a more attractive development site. 2- Removing the center median proposed for the area between the West Asheville-Riverlink Bridge and Lyman Street eliminates the need for grounds maintenance in the middle of a street and reduces the overall width needed as use for public right-of-way. Additionally, access to properties on either side of the road will be enhanced and pedestrian needs will be supported by a well-designed crosswalk t the roundabout.</p>
Blake Esselstyn		<p>On the whole, looks great. I think it's a good solution. The one piece that doesn't seem right for me given the alignment is that the inclusion of sidewalks on the east side of the road - this is from Amboy going up the the first privately-owned non-railroad parcels on the east side of Lyman. That stretch, having sidewalk there that is isolated between the road and the railroad property doesn't seem to make sense. I think that if money went into appropriate crossings with refuge islands or very safe crossings at Amboy and at the first appearance of development on the east side, the that area that otherwise would be devoted to sidewalk could be pervious surface to have a little bit more riparian buffer, a little more opportunity for a swale to alow stormwater to be absorbed. And I think folks would just prefer to walk on the river side any way as it is a more pleasant experience. And if there was more room for pervious surface for grass that could provide a little more space for the greenway if the whole thing could be shifted east. Similarly, north of the railroad trestle, south of hillcrest where the sidewalk would be between the road and the steep bank - if there was a good pedestrian crossing at Hill Street, and a nice pedestrian crossing at the south end of that stretch, maybe near the interstate bridge, the sidewalk on that side would not be needed. The pedestrian experience on that side would not be so pleasant and not so desireable and would be a lot of expense for the pervious surface.</p>
Adam Walters		1) I would realy like to see the installation of native plants (sp. grasses) on the banks and roadsides throughout the RAD green-ways. 2) Make a point to hire local designers, landscapers and plant suppliers for the contract work. We have so many talented up and coming businesses in Asheville that need continued support, the WD project coul be a significant bost to these local businesses.
Ava Carr	American Institute of Architects	Please address connectivity to West Asheville in the plan. Please address the connection to the Riverlink park / greenway system. Verify that all conditions are designed for use by large trucks used by the industrial businesses along the river.
Jim Stokely		<p>As a resident of Buncombe County and as one of Wilma Dykeman's two sons, I have a special interest in the Wilma Dykeman Riverway along the French Broad River. I feel confident that I can speak for my mother and say that, were she alive today, she would applaud the DOT's plan. She believed that the French Broad is not only a natural resource which should be preserved and enjoyed for its own sake, butalso that the river is a potential economic engine for the city, county and region. A fully implemented Riverway along the sections in question, as designed by the DOT, would lay the foundation for realizing much of that potential. We should be sensitive to the four businesses impacted by the DOT's plan. I hope that proper recompense or substitution can be agreed upon going forward in order to allow this project to proceed and succeed.</p>
Stuart Rohrbaugh	City of Asheville Development Services Department	<p>Newly aligned streets may require street naming or re-naming schemes for e911 address assignment purposes. Streets at each proposed roundabout intersection would normally continue the same street name through the roundabout intersection. The proposal appears to create a situation where Lyman Street may become "chopped" into different sections that could be subject to street renaming. For example: the roundabout at the 12 Bones site better realigns Lyman Street with Riverside Drive. The new alignment could allow the entire street segment along the French Broad River to be named the same street name. The section of Lyman Street south of the roundabout to the Amboy Road / Meadow Road intersection could be renamed Riverside Drive. The Riverside Drive street name would continue as the same street name through the proposed roundabout intersection. A preferred proposed street naming proposal could be initiated by the adjoining property owners prior to submitting plans for Final review.</p>

Sabra Kelley	12 Bones Steakhouse	<p>Thank you for reviewing the plans for the RADTIP project with us in more detail. We would like to take this chance to voice our concerns about how the project might affect our business, 12 Bones located on 5 Riverside Drive.</p> <p>These first issues relate to a scenario in which we remain in our present location 1. Loss of parking spaces for our restaurant: if the road goes through our lot as planned it will eliminate roughly 30 spaces and seems that these will not be created elsewhere (at least not close enough to our building to be usable by us) 2. Roundabout(s) installation: We already have problems when Norfolk Southern trains stop traffic; what happens when there are 2 roundabouts, one on either side of the train track?</p> <p>3. Relocation of our patio: cost/ disruption of business hours related to reworking our layout. Will we experience any problems with permitting due to our location in the flood plain (as we have experienced in the past?) 4. In general: Cost of changes to parking lot layout, plantings, sidewalks etc. Also disruption of business causing loss of income especially since we are only open when road crews work, Mon- Fri 11am-4pm</p> <p>These issues relate to a scenario where we are forced to relocate our business entirely (one that seems likely according to some of the NCDOT feedback we received): 1. Who pays for relocation? how is this expense estimated? does it include loss of income due to time spent on relocation from start to finish? 2. Will we experience any problems with permitting? 3. How do we determine where our business will be relocated to? Will we collectively agree on at least 3 options and then get to choose the one that works best for us? We understand there will be limitations but at the same time we are the ones forced to move so we feel that we should make the final decision.</p> <p>General thoughts:</p> <p>-How will the traffic from New Belgium affect all this, assuming they are done with construction before the RADTIP project is installed.</p> <p>-Ideas on historic designation: 5 Riverside Drive has hosted the President of the United States 2 times, we would argue that this fact is more important than the falling down, graffiti covered, brick shells across from us at JR Stone. Couldn't we look again at the curve that the proposed road follows? Also, if it is in fact possible to tear down the old Icehouse building then why not these "historic buildings"?</p> <p>-12 Bones opened in December of 2005, before a lot of people thought that the River District was cool. How come we get thrown under the bus? And what if the city relocates us to different part of town because we couldn't find a suitable location by the river. Then no one will be able to give directions to the RAD using our location as a reference point anymore.</p>
Richard Lee	EWANA Solutions Committee	<p>I'm told you're soliciting input on the Riverway project. To me it seems absolutely vital that replacing the span of the Norfolk Southern trestle bridge over Riverside Drive, along with widening the intersection of Riverside and Haywood, be included in the project. This would minimize the impact of truck traffic along the Riverway, particularly New Belgium Brewery traffic at the Riverlink Bridge/Haywood and on the Roberts-Lyman-Amboy secondary route. It would also reduce the overall distance traveled by trucks (or any vehicles) in River Arts. Thank you for the work you're doing. I appreciate it.</p>
Lyuba Zuyeva	Land-of-Sky Regional Council	<p>On behalf of FBRMPO Staff, I would like to comment that Wilma Dykeman RiverWay Study final design is in line with the local, state and regional policies which encourage complete streets and a multi-modal transportation system. Specifically, I would like to highlight that this project will be in line with the newly adopted NCDOT Healthy Transportation policy statement:</p> <p>"The North Carolina Department of Transportation will seek to have positive health outcomes by considering public health implications in our decision-making across all modes, programs, policies, projects, and services, and through all stages of the life of a transportation project from planning to project development, construction, operations, and maintenance. Specifically, we aim to improve public health outcomes through: optimizing a multi-modal transportation system to provide</p>
Darren Green	The Old Wood Company, 99 Riverside Drive	<p>Very excited about the RADTIP. My only concern is the proposed sidewalk on the east side of the road. The loading dock in front of the building is crucial to my operation. It must remain clear to allow trucks to pull in and out of the building. I have spoken to those on council and other committees and I have been reassured that this project will not interfere with the loading and unloading of trucks.. I appreciate this consideration in the design of that stretch of road.</p>
Dave Nutter	Nutter Associates, Community Planners & Development Professionals	<p>I thought the work was well done and should be approved.</p>
Kent Cranford	Motion Makers Bicycles, 878 Brevard Rd	<p>The plan looks great and I agree with the recommended alternative. I can't wait to see it in action!</p>
Jean Welsh		<p>Think the plan is good. Feel 12 Bones should be worked with to stay on river. They are established business and an asset to river.</p>
John Parrish	Asheville Parks and Rec Commission	<p>I think the project is fabulous and their presentation is very good - thorough and informative. The project will be a major asset for the City of Asheville.</p>
Jim Barham		<p>(1) widened roadway to make crossing RR tracks near Hill Street is an excellent plan. (2) It is not clear if crossing Riverside Drive from the [??] on bike traveling north beyond the RR bridge will be safe.</p>
Ava Carr		<p>Please help West Asheville access the Riverway (and all the Riverlink parks) safely.</p>
Pattiy Torno	CURVE studios & garden , 6-12 Riverside Drive	<p>I write in support of the RAD TIP preferred alternative presented by Land Design & Wilbur Smith Associates on December 6, 2012 @ the Dr. Wesley Grant Sr. Southside Center.</p> <p>As a member of the Wilma Dykeman RiverWay River Arts Community Advisory Committee over the past two years, I commend Stephanie Pankiewicz & all her partners in this project for their sustained, transparent & inclusive community involvement. I can comfortably say that all possible stakeholders were made aware of this process, were invited to join in, were heard in their concerns, with those concerns being taken into account.</p> <p>I stated early on that my goal was for our community to come up with the best possible scenario for a roadway that met the extremely diverse needs (bike lanes, sidewalks, greenway & parking as well as traffic lanes for a major north/south automobile artery) of our community. It was a priority for me to have my property's namesake, the curve, straightened out as well as the preservation of the building @ #14 Riverside Drive & #6-12 Riverside @ CURVE. Also, I saw the need to limit the scope of the roadway to allow for redevelopment opportunities where ever possible.</p> <p>In addition, I shared the desire for pedestrian access to our riverfront & pushed for the roadway come away from the river, which ended up being less possible than originally hoped. None of us got everything we wanted, which makes this a good document in my opinion.</p> <p>The current plan, adopted unanimously by Asheville City Council on December 11, 2012, is the best possible outcome for our community. It achieves all the transportation, historic preservations & environmental goals set forth by our community. Moving forward with this section of the Wilma Dykeman Riverway system will help our riverfront regain the prominence it enjoyed 100 years ago when my buildings were built.</p>
Meeting Attendee		<p>Craven Street Bridge MUST be improved to accommodate big trucks from New Belgium to turn onto Riverside Drive, Left (N) and Right (S). Excellent idea!!! No more tractor trailers going at Haywood Road all night long!!! Thank you-- except for Norfolk Southern Bridge, which is too low.</p>
Meeting Attendee		<p>240-retrofit like the James River at the parkway (ped walkway under existing bridge).</p>
Meeting Attendee		<p>Maintain bike lanes!! (debris from roadways)</p>
Meeting Attendee		<p>As much on-street parking as possible</p>
Meeting Attendee		<p>create romantic ruins</p>
Meeting Attendee		<p>Can we work with Norfolk Southern and create a crossing over the railline at Riverview Station (under/over), or connect Depot through to AbTech, where there is a bridge?</p>
Meeting Attendee		<p>Who is going to maintain all the trees and plantings</p>
Meeting Attendee		<p>One way pairs Roberts and Depot</p>
Meeting Attendee		<p>Thanks so much the layout was very user friendly. Thanks for sharing all of the info, great work.</p>
Meeting Attendee		<p>Very happy with proposed treatment crossing RR tracks.</p>
Meeting Attendee		<p>Study safety in bike rejoining road after RR bridge (Maybe Hill Street).</p>
Meeting Attendee		<p>This will be a great benefit to the quality of life when implemented. Consider a walking (unpaved) path between Craven Street Bridge and 12 Bones (along Lyman) as a temporary pedestrian solution -- very much needed judging by river concerts this summer.</p>

Meeting Attendee		In historic/business/more urban districts, on-street parking should be emphasized as much as possible. The purpose of on-street bicycle lanes is for commuting bicyclists (as opposed to recreational ones) to feel comfortable on the road. While in areas not conducive to redevelopment (ie. the industrial, flood plain areas to the north) might be best served by bicycle lanes and do not require on-street parking, the more dense and developed areas - WITH ON-STREET PARKING - will calm [??] traffic and create the desired effect. This reduces, of course the need for inefficient and wasteful value-reducing off-street parking in lieu of medians. The areas surrounding and directly east of '12 Bones' are a prime example of the location in which, paradoxically, bicyclists - as well as pedestrians, businesses and motorists - would be BETTER served by on-street parking with a bike share road rather than a bicycle lane.

RIVER ARTS DISTRICT TRANSPORTATION PROJECT

WILMA DYKEMAN RIVERWAY

Environmental Analysis, Project Development, and Preliminary Design

Project Newsletter No: 4 December 2012



Project Description

This project covers a 2.2 mile section of the Wilma Dykeman RiverWay, a 17 mile corridor along the French Broad River and Swannanoa River. This project will improve the transportation infrastructure in the River Arts District by improving Riverside Drive and Lyman Street, adding measures to control stormwater, constructing greenways (multi-use trails), adding sidewalks, constructing turn lanes at intersections (as needed), and possibly adding other amenities such as on-street parking, transit stops, and landscaping. The specifics of the project will be determined following an analysis of project alternatives in phase I.

The project is currently funded for Environmental Documentation, Alternatives Analysis, and Preliminary Engineering. Activities underway for 2011 – 2012 will include collection of environmental data, inventory of existing conditions, traffic analysis, alternatives development, study of environmental impacts, public involvement, selection of preferred alternative, NEPA approvals, 30% preliminary design drawings, preliminary cost estimates, implementation plan, and preliminary right-of-way estimates. The consultant team will be responsible for the Federally Approved NEPA document – a Categorical Exclusion – and Preliminary Design.



Purpose and Need



The primary purpose of the project is to improve existing roadway geometric deficiencies along Riverside Drive and Lyman Street in the River Arts District and to enhance the multimodal mobility and system linkages (vehicular, pedestrian, bicycle, and transit) by providing efficient and convenient access from Amboy Road to Hill Street.

These improvements will enhance safety and provide additional modal options for the traveling public and visitors to the River Arts District. These modal and service connections will improve regional mobility, particularly for pedestrian and cyclists.

Project Goals

- ❖ Be consistent with previous plans (Asheville 2025 Plan, Wilma Dykeman RiverWay Master Plan and other City plans)
- ❖ Create a Unique Gateway for the City of Asheville
- ❖ Connect adjacent neighborhoods and downtown to each other and the French Broad River
- ❖ Promote Smart Growth, including Mixed Use and a Range of Housing Options in the River Arts District
- ❖ Increase Multimodal Transportation Choices – transit, bike, pedestrian and river access
- ❖ Enhance Economic Competitiveness and Promote Sustainable Redevelopment
- ❖ Provide Recreational Opportunities for Residents and Tourists
- ❖ Improve Environmental Protection and Water Quality of the French Broad River
- ❖ Develop Opportunities for Streetscape, Amenities and Public Art
- ❖ Minimize Conflict Points for Cyclists and Pedestrians
- ❖ Preserve Future Mobility through Access Management





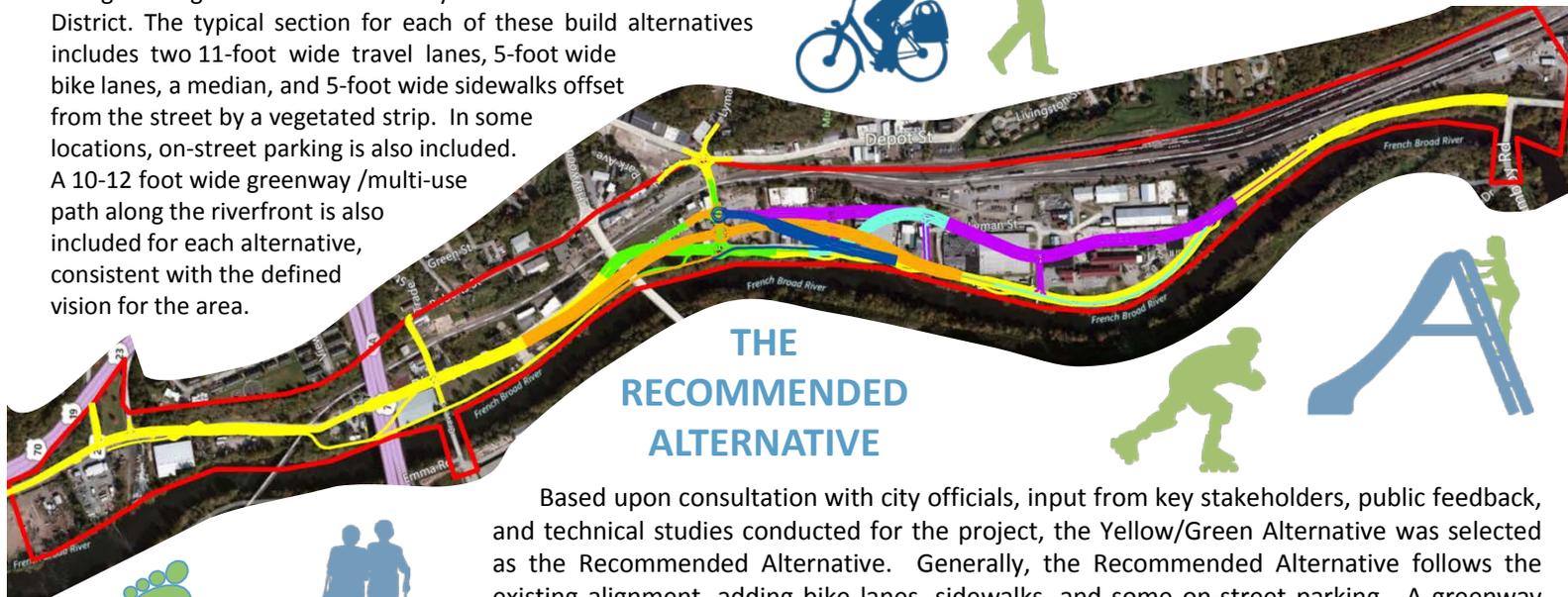
ALTERNATIVES CONSIDERED

A number of alternatives were considered to address the purpose of and needs for the project: Low cost measures that reduce travel demand to improve traffic flow, Mass transit, Relatively low cost measures that improve traffic flow by increasing network efficiency, Improving the existing facility, and Constructing alternatives on new alignment. The No Build Alternatives was also considered. Most of these options were dismissed early in the process because they would not fully satisfy the project purpose.

A variety of build alternatives were developed to improve existing geometric deficiencies and enhance multimodal mobility/system linkages along Riverside Drive and Lyman Street in the River Arts District. The typical section for each of these build alternatives includes two 11-foot wide travel lanes, 5-foot wide bike lanes, a median, and 5-foot wide sidewalks offset from the street by a vegetated strip. In some locations, on-street parking is also included. A 10-12 foot wide greenway /multi-use path along the riverfront is also included for each alternative, consistent with the defined vision for the area.



Build Alternatives considered (below) and Sample cross-section (above)



THE RECOMMENDED ALTERNATIVE

Based upon consultation with city officials, input from key stakeholders, public feedback, and technical studies conducted for the project, the Yellow/Green Alternative was selected as the Recommended Alternative. Generally, the Recommended Alternative follows the existing alignment, adding bike lanes, sidewalks, and some on-street parking. A greenway multi-use trail would run between the improved roadway and the river for the length of the project. The next page discusses why Yellow/Green was recommended...



FINAL DESIGN & PROJECTED COST

Once the NEPA document is finalized later this month, final design work can begin in 2013. As part of the final design phase of work, designers will take 30% plans for the Recommended Alternative's roadways, sidewalks, greenways/multi-use paths, and other improvements and will develop additional details. This next phase of work will address landscaping, public art, stormwater, coordination with utility companies and the railroad, and more.

Preliminary cost estimates are presented in the table below. These costs will be updated as final designs are developed, which will include more precise estimates for construction, utility relocation, right-of-way acquisition, and more. At this time, future funding for construction has not been identified.

Recommended Alternative	Projected Cost*
Roadway and Structures Construction	\$12.8 million

*Note: This cost does not include Right of Way and Utility relocation costs



RIVER ARTS DISTRICT TRANSPORTATION PROJECT

A Section of the Wilma Dykeman Riverway

The following table summarizes impacts resulting from the Recommended Alternative. These will be presented in more detail in the Categorical Exclusion Report for the project, which will be completed by the end of the year.

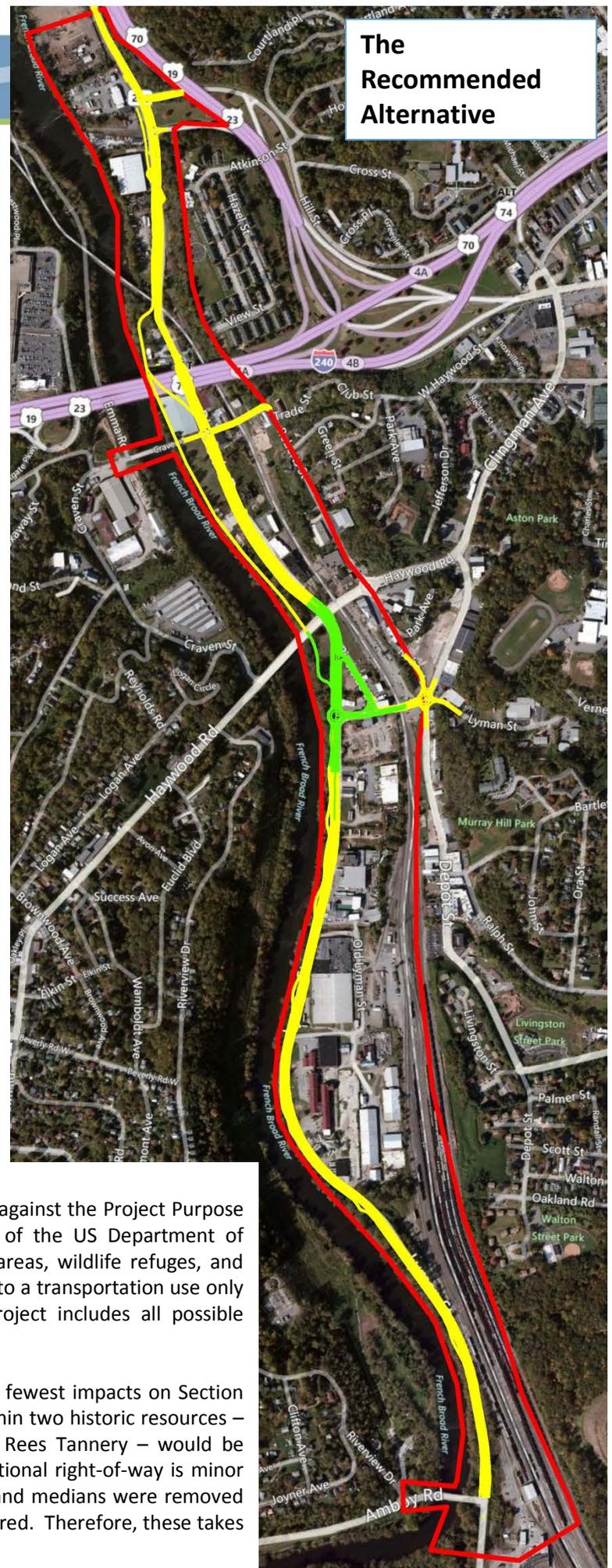
IMPACT CATEGORY	DESCRIPTION
Plants & Animals	<ul style="list-style-type: none"> Unlikely to impact Threatened or Endangered Species Minor potential to impact other species
Water Resources	<ul style="list-style-type: none"> Impacts to streams due to replacement of pipes and culverts Project lies within floodplain & floodway; permits will be required
Historic Resources	<ul style="list-style-type: none"> Minor impacts on the Riverside Industrial Historic District Minor impacts on the former Hans Rees Tannery (historic site) Archaeological monitoring during construction
Community Resources	<ul style="list-style-type: none"> 0.05 acres of right-of-way within Jean Webb Park and 0.70 acres of right-of-way within city-owned parcel south of Jean Webb Park, planned for future park use No residential relocations Four business relocations
Hazardous Materials	<ul style="list-style-type: none"> Additional right-of-way required from 19 parcels with known contamination
Land Use	<ul style="list-style-type: none"> Project supports ongoing redevelopment efforts
Visual	<ul style="list-style-type: none"> No perceptible impacts
Economics	<ul style="list-style-type: none"> No perceptible impacts
Noise	<ul style="list-style-type: none"> No perceptible impacts

WHY IS YELLOW/GREEN THE RECOMMENDED ALTERNATIVE?

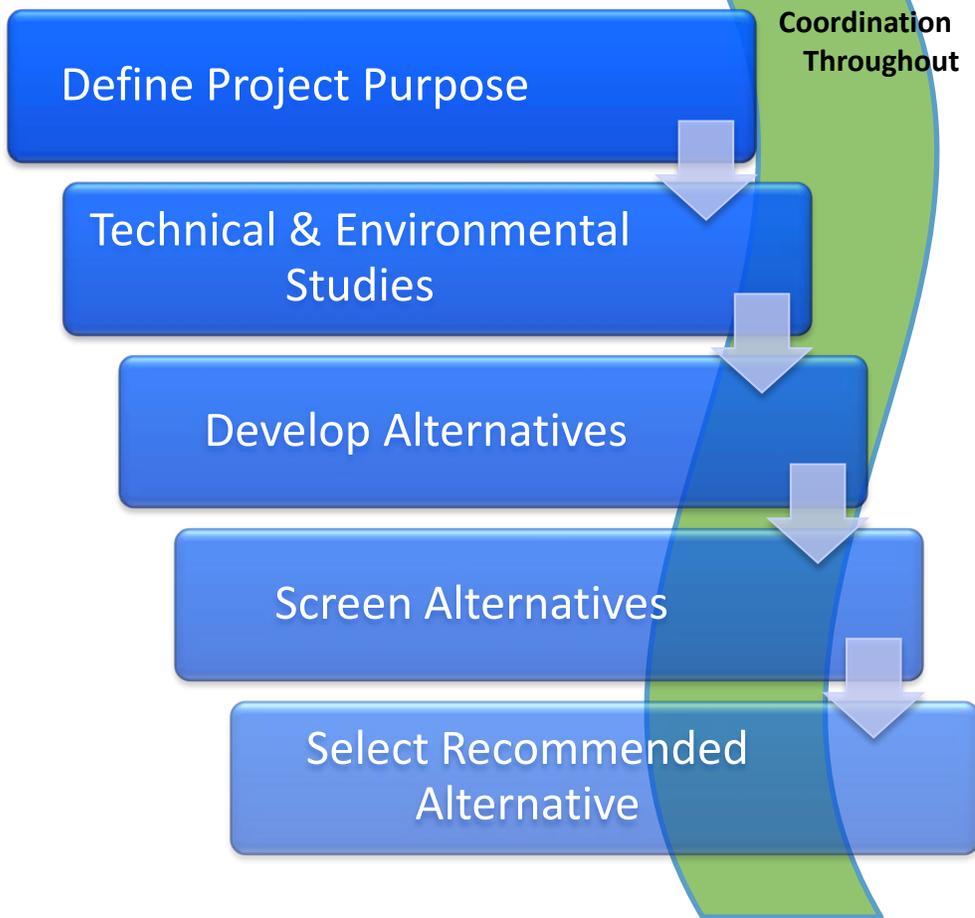
All the alternatives shown on the previous page were screened against the Project Purpose and Need then against federal law. Specifically, Section 4(f) of the US Department of Transportation Act protects publicly owned parks, recreation areas, wildlife refuges, and historic sites. By law, a Section 4(f) property may be converted to a transportation use only if there is no prudent and feasible alternative and if the project includes all possible planning to minimize harm to the resource.

Of all the alternatives considered, Yellow and Green led to the fewest impacts on Section 4(f) resources. However, they do still lead to impacts. Land within two historic resources – the Riverside Industrial Historic District and the former Hans Rees Tannery – would be converted to a transportation use. At both locations, the additional right-of-way is minor and located adjacent to existing roadways. On-street parking and medians were removed from the proposed cross-section to minimize the new area required. Therefore, these takes count as *'de minimis'* or minor uses.

The Recommended Alternative



MAJOR STEPS IN THE PROCESS



THANK YOU for your input throughout this phase of the project. By sharing your knowledge of the River Arts District and your planned vision for the future, the project team has been able to develop a range of alternatives that reflect the community's values. A special thanks to those groups who partnered with the City:

1. **Steering Committee** is made up of City Staff, RiverLink and NCDOT.
2. **Advisory Committee** consists of a range of civic and community leaders and advocates.
3. **Stakeholders** represent property owners, artists, businesses and community groups within or adjacent to the study area
4. **Members of the Public** who attended presentations and provided input
5. **State and Federal Resource Agencies**



More Questions or Comments? If so, please contact **Dan Baechtold** with the City's Transportation Department. Please submit any comments by **December 21, 2012**.

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