

City of Asheville

River District Design Review Committee

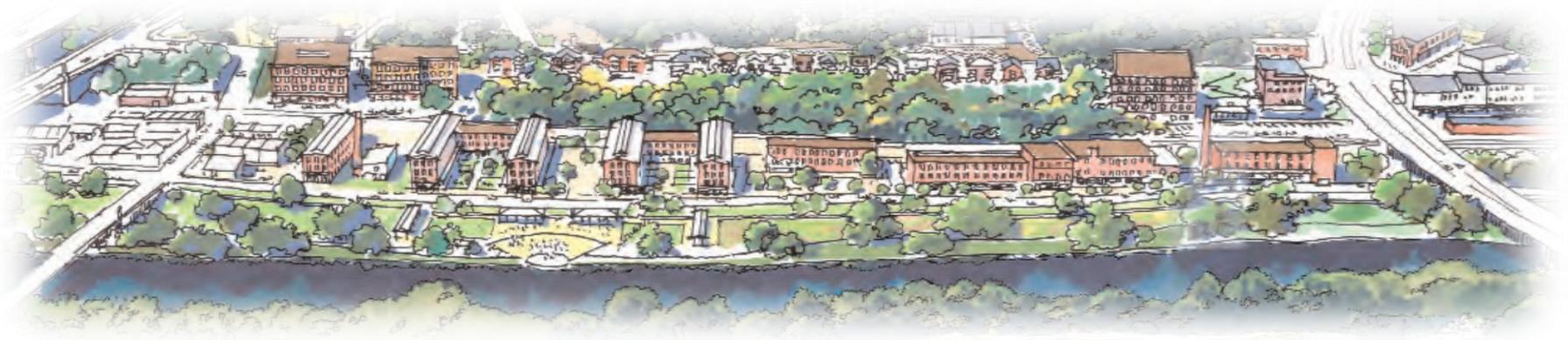
# Design Guidelines



## Forward

It is the intent of this document to serve as a guide for architects, planners, designers, developers, and property owners - to provide understanding and inspiration for their project proposals along the riverfront. These guidelines are not intended to dictate a certain style, nor develop a “cookie-cutter” approach to design features. On the contrary, it is hoped that the images contained herein, will be a springboard for new and creative approaches that respect the historical character of the area, be complimentary to the surroundings, and reflect the authenticity of the Riverfront. With this in mind, it is expected that this process will result in projects that are successful and which will be an important addition to the fabric of the Riverfront.

Asheville is a unique city, and the Riverfront reflects that same quality of uniqueness. We encourage early discussions with City Staff and the River District Design Review Committee, to gain a better understanding of the Riverfront and the process which will affect your particular project.



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## Introduction

As a result of the adoption by the Asheville City Council of the Unified Development Ordinance and the requirements for architectural review by the River District Design Review Committee, the following guide-lines are enacted for the architectural review of all construction work requiring building, sign, demolition, and zoning permits, and projects requesting a variance, which are located within the River District.

The following River District Design Guidelines are intended to compliment the Unified Development Ordinance of the City of Asheville. They are not intended to replace or circumvent the requirements of that or any other ordinance of the City of Asheville. Where the Guidelines conflict with a City of Asheville ordinance, the ordinance shall prevail.

As a result of the Asheville City Council's adoption of the Riverfront Plan in 1989, and therefore its status as an official policy document of the City of Asheville, it is understood that the intent of the City in the River Core District north of Lyman Street and south of the Norfolk and Southern Rail Road trestle, and east to the structures on Roberts Street, is primarily to encourage use of existing structures as artist's studios, and residences, as well as retail and whole sale space for the products produced in those studios. Restaurant, festival, and nightclub uses are also encouraged. It is also understood that there are many sites within this area where further development will take place. It is contemplated that these areas will primarily serve as retail space for arts and crafts, many of which will also be produced in the immediate

areas bordering the French Broad River. The structures in this area tend to be clustered in a discernible district and at a pedestrian scale. This district is in the process of transition from warehousing and very light industrial uses to a series of retail commercial, arts production (studios), and festival uses. It has the opportunity to be developed in such a way as to create internal, non-linear pedestrian connections (sidewalks, paths, etc.) from structure to structure and from property to property which do not run along Riverside Drive. This will allow the development of the district in this area in a way that will separate pedestrian traffic from high speed vehicular traffic, and especially from the truck traffic generated by the uses south of this area.



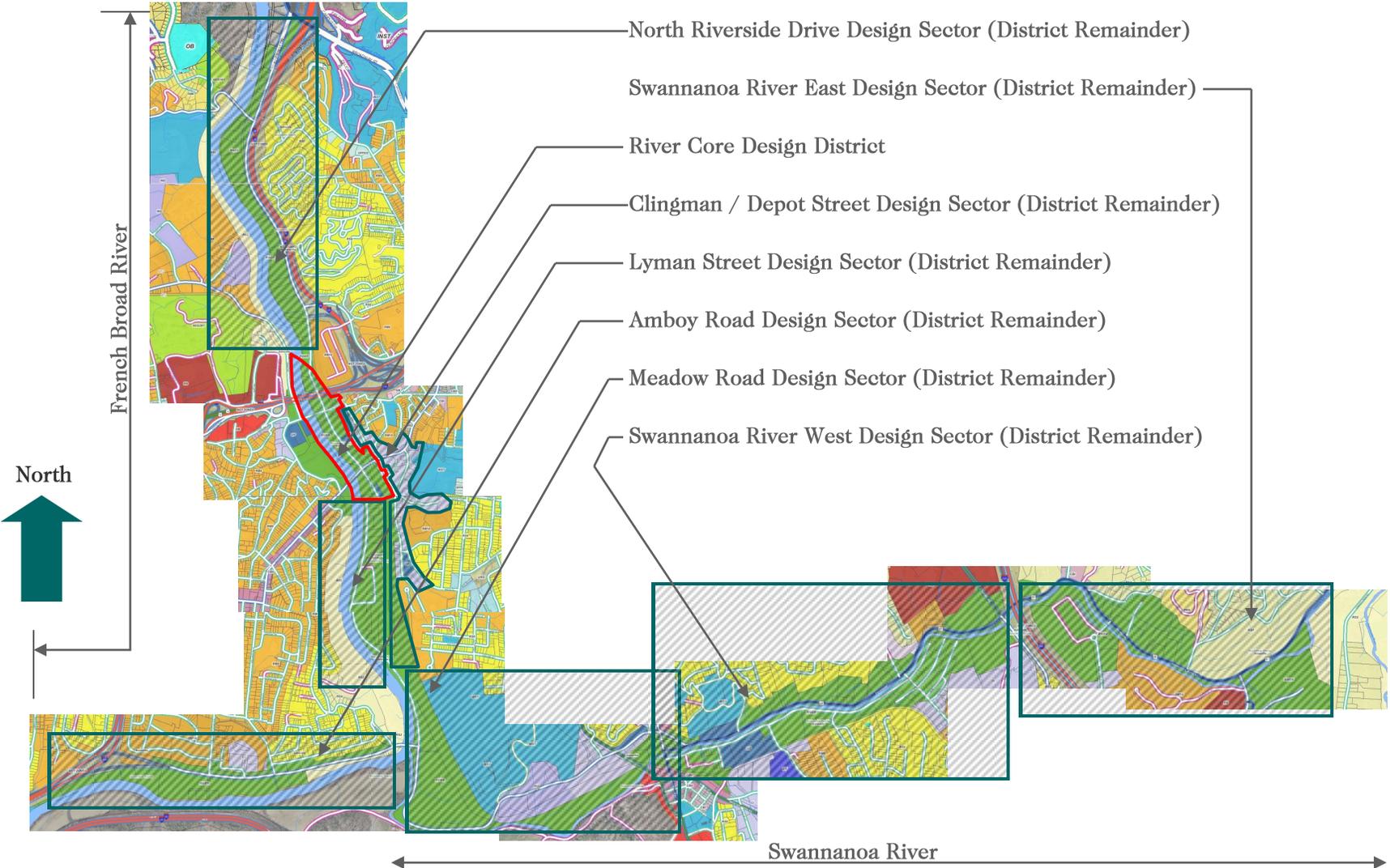
The ability to discern the existing cluster of uses as a district is reinforced by a style and look in the area, that although allowing for fairly broad individual expression, also conforms to a look and feel which, if lost, will lessen the perception of the space as a unified whole. This loss would be detrimental to the ability of the area to market itself to one of its largest potential customer segments, shoppers, primarily but not exclusively tourists, at the retail level. This look and feel derives from the preponderance

of structures in the area which were built during the late 19th and early 20th centuries. These structures, usually of a red brick, and of an industrial design from that period, are slowly being converted into studios and residences.

The viability of this area as a cultural center is further reinforced by the current project in the West-End Clingman area to restore the residential neighborhood. This affords the opportunity to create housing for people working in the district, should they so desire. It is also acknowledged that, for the near future, the land along the French Broad River south of Lyman Street, down to Amboy Road will remain in its present use pattern, primarily light industrial, and as such will create the need for design standards geared more towards those uses. North of The Norfolk and Southern rail trestle, the land uses tend to be industrial, and linear in nature and pattern, rather than clustered in such a way as to create a small pedestrian oriented district and as such will create the need for design standards geared more towards those uses.

The uses along the Swannanoa River tend to be industrial and commercial in nature. The design concerns of these areas are different from the River Core Area described above, and require that design issues in these areas be addressed separately. Therefore, pursuant to the creation of an architectural review process in Chapter 7 of the Code of Ordinances of the City of Asheville, the following architectural and design guidelines are created for two areas: (1) the River Core Design District and (2) the Remainder of the River District.

# Riverfront Areas Map



## River Core District

This is the core area of the River District. As such, it merits a special set of design guidelines. The core area is defined on the attached map (Attachment A) titled “River Core Design District”.

### Character Buildings

Character Buildings are those buildings which, by virtue of their similarities in construction type, age, historical significance, and visual characteristics as well as their locations throughout the district, set the architectural character of the Riverfront Design Core District. These buildings are listed in Attachment B, Character Buildings. Restoration, renovation, and creative use of these buildings in a manner which would retain their unique character as shown in the accompanying photographs is strongly encouraged. Demolition or exterior alteration in a way which would destroy their original character is strongly discouraged. While compatibility with the context of the area is extremely important in order to retain the identity of the River Core area as a distinct district in the City,



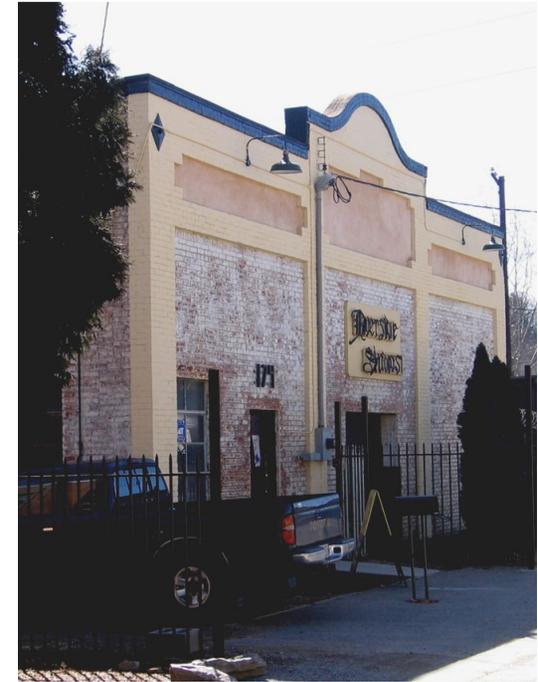
it is conceivable that a design which does not meet the criteria for Character buildings and Building Facades may be submitted. Such a building can be considered for approval if it meets the following criteria:

1. It is the opinion of the River District Design Review Board that the proposed structure will further the identity of the River Core Area without creating conflict in the public perception of the area as a unique element in the city; That the structure exhibits special creativity which furthers the goals and intents of the Riverfront Plan and visually unites the surrounding structures.
2. The design conveys unique qualities of the River Core Area heritage, and local craftsmanship, by drawing heavily on the various products, arts and crafts which typify now or historically, items produced in the River Core Area.
3. The design uses elements of the character buildings in creative ways which maintain compatibility with the 4. character, materials, and design of the Character Buildings referred to above.

### Building Facade

All buildings in the Riverfront Design Core District should be faced with materials prevalent in nearby Character Buildings. Facades should incorporate elements found in Character Buildings in the immediate area.

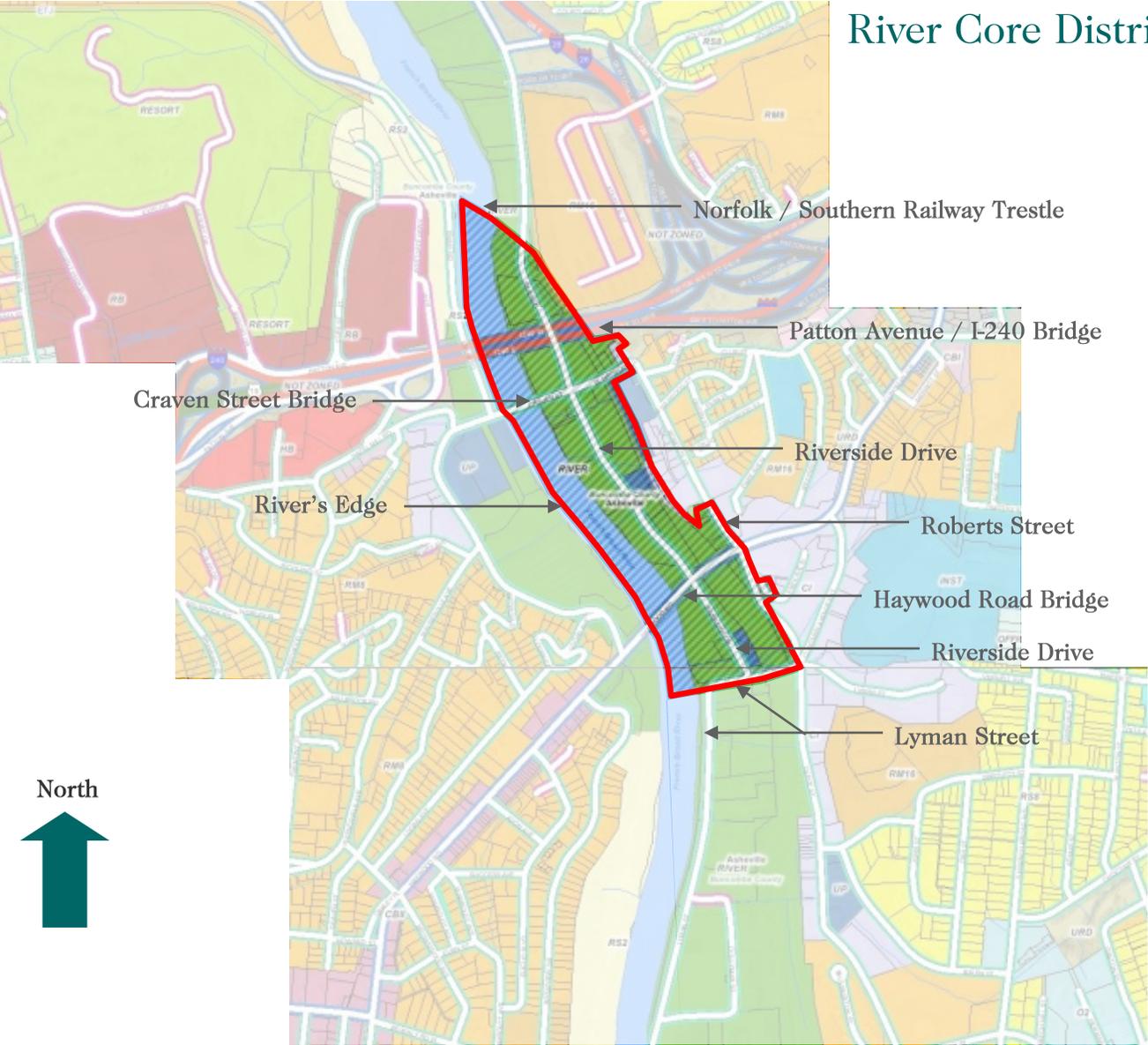
West of the railroad tracks this will mean red brick, with architectural insets (refers to the practice of setting large areas of wall back from other surfaces on the wall to create a suggestion



of columns), designs in the brick (out of both brick and other material, usually very geometric), brick arched headers over the punched window openings, coping at the roof level, and flat roofs, possibly with parapets. Architectural details should complement the neighboring Character Buildings. Bricks should not be painted. East of the Railroad tracks and north of Haywood Street, the exterior standards will be as above.

East of the railroad tracks, and south of Haywood Street, this will mean either bare brick, or painted brick. Colors should be organic and muted in keeping with the existing Character Buildings in the immediate area.

# River Core District Map



## Site Plan

Buildings West of the railroad tracks should be sited so that they are open to Riverside Drive and the French Broad River. They should not be clustered in such a manner as to isolate themselves, their users or their occupants from either the street, the river, or their neighbors. They should be arranged in a non-linear pattern, with varying setbacks from the road, and in keeping with the atmosphere created by the placement of other nearby Character Buildings. It is the intent of the officially adopted Riverfront Plan that the land immediately adjacent to the river be open, public festival space. Thus all site plans should incorporate that concept into their design, such that they reinforce the festival space, and are in turn reinforced by it.

East of the railroad tracks and south of Haywood, Road, the buildings should be more linear and built up to, or very close to the sidewalk or public right-of-way line, while still respecting the requirements of the Unified Development Ordinance. The intent is to build upon the ambiance of this area by continuing the patterns started by the placement of the Character Buildings. These patterns differ slightly from the character created by the placement patterns of the area west of the railroad tracks.

Throughout the district, open spaces between buildings should be put to use as parks, recreation space, and passive space available to residents and pedestrians for relaxation.

## Windows

Windows and doors should be of an appearance, size and placement in keeping with the type, number and placement of windows in the most visually dominate Character Building in the



immediate area of the proposed building. Windows should not be placed in long strips, but should be spaced in keeping with the rhythm of the nearest Character Building. Windows in existing buildings should be replaced in keeping with the visual character in the surrounding Character Buildings. The intent of this section is to create a continuous and unified visual image that amplifies and promotes the character of the Riverfront Design Core District as exemplified in the Character Buildings.

However, originality in the use of color and window planting boxes is encouraged. The use of tiles and color is encouraged, as is the use of materials, goods and services produced in the Riverfront District. A unified color scheme incorporating the window sills, frames or mullions is encouraged. Window boxes which, with their contrast to the industrial architecture, will add color and organic form are strongly encouraged.

## Awnings

The use of Awnings is encouraged. The material should honor the history of the old cotton mill and

should be of a material appearing to the eye as cotton or similar fabric. Colors should be in keeping with the muted nature of the use of red brick in the majority of the Character Buildings.

## Service and Delivery Areas

All service areas (those spaces accommodating dumpsters, trash cans, HVAC, etc.) should be screened and located between buildings, or at the rear of buildings. They should be aggressively screened from neighboring views by the abundant use of vegetation which will perform this function during all seasons of the year. Delivery Areas (those spaces used for delivery of goods or services for the occupant of the building) should be aggressively screened from adjacent uses, pedestrian walkways, and the street through the abundant use of vegetation which will create a screen during all seasons of the year.

## Internal Site Illumination

The use of indirect techniques such as up-lighting is encouraged. This will help soften the area and define a unique environment in the Riverfront District. A single unified scheme throughout the

Riverfront District is desired. Pole lighting is discouraged. The River Design Review Committee will specify a preferred lighting fixtures, housings, and lamps. These specifications are included as Attachment C

## Signs

Sign numbers and size are regulated by Article 13 of the Unified Development Ordinance. Recognition and preservation of the old original signs painted on the sides of is strongly encouraged.

Originality is encouraged. The use of tiles, color, sculpture, paintings, mosaics, etc. is encouraged as is the use of materials, and goods produced in the Riverfront District. The use of corporate or commercial logos, or cartoon characters associated with a commercial enterprise, is discouraged.

The intent is that signs express the nature of the business in the building, and that they graphically portray such whenever possible. The signs should not detract from the character of the building, or its design nuances nor should the sign be out of character with the building in terms of size, color, form, mass, or placement. Instead signs should compliment the building facade, and add to the overall impression of the building while reinforcing the building's design. Signs should be placed within architectural features whenever possible. Sign materials should be compatible with the facade materials. Signs may be mounted flush with the building's facade, or project at right angles. External illumination is strongly encouraged. Internal illumination is very strongly discouraged. Pole signs are also very strongly discouraged. Free standing ground level signs are acceptable. Materials should be organic, and complement the character of the buildings in the immediate area.



All freestanding signs should conform to the standards, beginning on page fifty six of the Asheville Riverfront Open Space Design Guidelines. A copy of this section has been included as Attachment D.

## Sidewalks

All sidewalks should be concrete with simple score patterns, preferably creating squares. This uniformity will add to the sense of unity within the district. Originality in the use of patterns, original granite curbing, tiles, color, sculpture, paintings, mosaics, tree and planting wells, etc. within the context of the uniform sidewalk standard above, is encouraged, as is the use of materials, and goods produced in the Riverfront District. The use of corporate or commercial logos, or cartoon characters associated with a commercial enterprise, is discouraged.

Sidewalks in any but the area of Roberts Street and Clingman Avenue, should be non-linear, and should connect to the edge of the property or

development so as to tie into the sidewalks of the neighboring development. Although the sidewalks should not approach the rail tracks, neither should they simply parallel the street. The intent is to create a unified district with pleasant internal circulation that avoids mixing pedestrians with street traffic as much as possible. As such, sidewalks may be an integral part of a circulation pattern through a given site, and may pass through malls, gardens, etc., and bear no direct relationship to the street, except, where appropriate, provisions should be made for connecting with pedestrian entrances to the river front festival area.

Roberts Street, north and south of Haywood Street, and Clingman Avenue in the commercial area, form an urban sub-district in the river front area serving as a transition area between the downtown commercial districts at the top of Clingman Avenue and the mall nature of the areas west of the railroad tracks. Although retaining the density and spaces characteristic of an urban environment, this area is more directly related in architectural character, history, current use, and economic functioning to the riverside area west of the railroad tracks. Thus some design elements require similar but slightly differentiated treatment from the same elements in the riverside area west of the railroad tracks. One of these elements is sidewalks. Sidewalks along Roberts Street and Clingman Avenue should conform to the design guidelines above, except that they should be linear, and located immediately along side of the public streets unless they detour away from the street to allow the placement of a tree well, or other street furniture (including Sculpture) that serve to enhance the area and the relationship between the building and the street.

## Parking

Originality is encouraged. The use of patterned paving, bricks, original granite curbing, tiles, sculpture, paintings, mosaics, tree wells, etc. in the paving and around the parking area is encouraged, as is the use of materials and goods produced in the Riverfront District. The use of corporate or commercial logos, or cartoon characters associated with a commercial enterprise, is discouraged. The creative use of landscaping is strongly encouraged.

The number of parking spaces should be minimized. If remote or shared parking is available, it should be utilized to the maximum extent possible. The parking areas should be located such that they have a minimal conflict with foot traffic in the area, yet relate to it closely. All parking areas should be pleasant and feel inviting. In consideration of the fact that the entire Riverfront District is designed as a pedestrian complex, no parking area should directly abut the street, and plantings should be used between the street and the parking area such that any view of automobiles is filtered. It is preferable if nothing below the bottom of the windshields is visible. These same standards should be applied to parking areas and their relation to interior spaces. Shade trees appropriate for the Riverfront District, should be planted so that the lots remain cool and pleasant. Up-lighting the trees and surrounding buildings to create a unique environment and help definition and safety at night is strongly recommended.

Parking lot lighting should be considered internal site lighting, and should conform to that section of these guidelines.

## Utilities

All utilities should be placed underground.

## Street lighting

All street lighting and street lighting fixtures should be of a uniform character complimenting the character of the Character Buildings (See attachment C). Public street lighting should be chosen in conjunction with the needs and requirements of the City of Asheville Public Works Department.

## Street Furniture

The use and placement of public furniture such as benches, trash receptacles, tables, planters, drinking fountains etc. is encouraged. Originality is encouraged, but an overall sense of visual unity within and between individual projects is desired. The use of color, tiles, sculpture, paintings, mosaics, etc. is encouraged, as is the use of materials and goods produced in the Riverfront District. The use of corporate or

commercial logos, or cartoon characters associated with a commercial enterprise, is discouraged. The creative use of landscaping is also encouraged.

Unless the particular piece of furniture is a unique original piece of art, it should respect and compliment the ambiance established by the Character Buildings in the area or by the Festival spaces along the French Broad River. If the furniture is an original and unique piece of art, it should respect the character of the area, and enhance the festive feeling being created in the area.

## Sculptural Art

The use of sculpture, fountains, etc. is strongly encouraged. Originality in materials, placement and execution is also highly encouraged. The use of materials and goods produced in the river front area is strongly recommended.



## Other

The placement of Kiosks is highly encouraged. Treatment of the kiosks as public sculpture enhancing the character of the area is also encouraged.

Larger roofed but non-walled spaces designed to be entered should conform to historical patterns in the area that are compatible with the character of the area. Liberal use of landscaping to define these areas is encouraged. The intent is to create public service areas that will visually compliment the design guidelines being applied in the Greenway and Festival space along the French Broad River (See the Asheville Riverfront Open Space Design Guidelines, Attachment D).

Structures built in or built to appear as though they are in the French Broad or Swannanoa Rivers should conform to all the above guidelines for all parts of the project which may be on land except as noted below. Docks not originating in the planned open space areas of the Riverfront District should honor the history of the area in materials, form, size, and color, as should any structure associated with the docks, either directly over water, or in close proximity to the docks. If the use of the project is to be primarily entertainment or water oriented uses, then it is possible that a more appropriate character building would be those structures used in the early part of the 20th century in the recreational parts of the river below Richmond Hill. Photographs of these uses are available, and they are the basis of many of the structures detailed in the Asheville Open Space Design Guidelines. Any structure connected with the land bordering the French Broad River which is also in the



planned open space areas of the River District should conform to the Asheville Riverfront Open Space Design Guidelines.

Structures in the River proper should conform to the above guidelines unless constructed to appear as a boat. If the structure resembles a boat, then it should have a direct visual relationship in size, color, and shape to historical boat styles as have been used on the French Broad River. If an actual

water craft is used for commercial, residential or decorative purposes and is in the French Broad River or used in extremely close proximity to the River such that it appears to be in the French Broad River, it should have a direct visual relationship in size, color, and shape to water craft actually used on the French Broad River in times past. A festive and inviting atmosphere celebrating the river and its past is encouraged.

## The Remainder of the River District

The roads along the French Broad and Swannanoa Rivers are used by local residents for access to many different places in the City. These include the University of North Carolina Asheville, the Town of Woodfin, I-240, the Asheville Downtown area, The Riverfront Core Area, the French Broad Riverfront Park, Amboy Road, the Biltmore Village Area, the businesses along the Swannanoa River, the Tunnel Road Corridor and the industrial and commercial areas just east of I-240 on the east side of the City.

These same roads are used as industrial and commercial roads, and as such, access many business and industrial uses along the French Broad River. The area along the French Broad and Swannanoa rivers, is, for the most part, linear, and differs in form and current use from the Riverfront Design Area.

Some parts of Riverside Drive, Meadow Road and Swannanoa Road are heavily used by automobile and truck traffic. These roads also play a vital part in the transportation system of the City. The uses along these roads no longer generate only industrial and heavy commercial traffic. Certain of the areas along these roads are slowly entering transitional phases from industrial and light industrial to retail commercial. Although, the buildings are usually utilitarian in structure, the uses along the French Broad and Swannanoa Rivers are able to contribute to the overall visual quality of the City and visual enhancement of the area with minimal effort.

For these reasons, and pursuant to the creation of an architectural review process in Chapter 7 of the UDO, the following architectural and design guidelines are created and apply to all that area zoned River in Chapter 7 of the UDO except that area described above as the Riverfront Core Area.

### Industrial Uses

For the purposes of River District Design Review, Industrial Uses are those uses allowed in the Industrial District of the Unified Development Ordinance (UDO), and in the Office/Business and Industrial categories of the Commercial Industrial District in the UDO. In the event a use is not listed as an allowed use in the Unified Development Ordinance the Architectural Review Board will decide whether the use is to be considered Industrial or Commercial for design review purposes.

Where possible, the site plan should include a

grassed front yard. Fencing or landscaping should delineate the front yard from the plant site. The intent is to create a visual environment that is fun, attractive, and even occasionally whimsical in nature to complement and offset, where possible, the serious work that takes place in production facilities, and to create a public appreciation of the industrial facility as a positive member of the community.

Where new buildings are built, a distinctive character honoring the older and longer standing architectural traditions of the river front areas is encouraged.

All parking areas should be buffered from the road with landscaping. Parking for plant employees should be located on site, and not in the front yard area. It is recommended that visitor parking be located so as to minimize conflict with plant operations. If visitor parking is





located in the front yard area, the parking areas should be buffered from the road by the use of landscaping which will provide an effective screen through all the seasons of the year. Landscaping, preferably trees, should be used to visually break up any expanses of building wall, especially where the walls have few or no windows.

Elevated entrance lighting should be mounted on appropriately designed poles, such as aluminum poles with a brush finish.

Long runs of fence should be broken up with large growing evergreen shrubs or trees. Yards which run along the French Broad River or the Swannanoa River should be left as pristine as possible in the proximity of the River, screening the industrial uses from the view of those using the river.

Rail crossings, where applicable, should be well marked, and, where passing through front yards, landscaped in an attractive manner, when possible.

All building facades facing Riverside Drive or Swannanoa River Drive, and all front yard trees, should be illuminated where possible, preferably using up-lighting.

If buildings are painted, they should be in muted organic colors that do not in themselves attract attention or overpower the natural colors of the river environment.

Owners, engineers or designers should consider up-lighting any towers, scaffolding or other industrial equipment that may also present a sculptural image. Creativity in the use of lighting and color is encouraged.

The use of sculpture, fountains, etc. is strongly encouraged. Originality in materials, placement and execution is also highly encouraged. The use of materials and goods produced in the river front area is strongly recommended.

### Utilities

All utilities should be placed underground whenever possible.

### Signs

Originality is encouraged. The use of tiles, color, sculpture, paintings, mosaics, routed wood, etc. is encouraged, as is the use of materials, goods and services produced in the Riverfront District. External illumination is strongly encouraged. Internal illumination is very strongly discouraged.

All freestanding signs should conform to the standards, beginning on page fifty-six of the Asheville Riverfront Open Space Design Guidelines.



## Other

Structures built in or built to appear as though they are in the French Broad or Swannanoa Rivers should conform to all the above guidelines for all parts of the project which may be on land except as noted below. Docks not originating in the planned open space areas of the Riverfront District should honor the history of the area in materials, form, size, and color, as should any structure associated with the docks, either directly over water, or in close proximity to the docks. If the use of the project is to be, primarily entertainment or water oriented uses, then it is possible that a more appropriate character building would be those structures used in the early part of the 20th century in the recreational parts of the river below Richmond Hill. Photographs of these uses are available, and they are the basis of many of the structures detailed in the Asheville Open Space Design Guidelines



Any structure connected with the land bordering the French Broad River which is also in the planned open space areas of the River District should conform to the Asheville Riverfront Open Space Design Guidelines.

Structures in the River proper should conform to the above guidelines unless constructed to appear as a boat. If the structure resembles a boat, then it should have a direct visual relationship in size, color, and shape to historical boat styles as have been used on the French Broad River. If an actual water craft is used for commercial, residential or decorative purposes and is in the French Broad River or used in extremely close proximity to the River such that it appears to be in the French Broad River, it should have a direct visual relationship in size, color, and shape to water craft actually used on the French Broad River in times past. A festive and inviting atmosphere celebrating the river and its past is encouraged.

## Commercial Use

For the purposes of River District Design Review, any use not classified as an Industrial use shall be reviewed under the standards for Commercial Uses.

The adaptive reuse of existing structures is recommended for all buildings with historical character. The adaptive reuse of sheds, metal buildings etc., while acceptable, is discouraged. Where new buildings are built, a distinctive character honoring the older and longer standing architectural traditions of the riverfront areas is encouraged. For new and existing buildings, reference may be made to the Character Buildings in Attachment B. or other historical documents, including photographs, available



through the City of Asheville Planning and Development Department.

## Site Plan

Buildings should be located on the site such that they are open to and relate to the River. They should not be clustered in such a manner as to isolate themselves, their users or their occupants from the street, the river, or their neighbors.

Throughout the River District, open spaces between buildings should be put to use as parks, recreation space, and passive space available to residents and pedestrians for relaxation.

## Service and Delivery Areas

All service areas (those spaces accommodating dumpsters, trash cans, HVAC, etc.) should be discreetly placed. This means they should be screened and located between buildings, or at the rear of buildings. They should be aggressively screened from neighboring views by the abundant use of vegetation which will perform this function during all seasons of the year. Delivery Areas (those spaces used for delivery of goods or services for the occupants of the building) should be as discrete as possible. They should be aggressively screened through the abundant use of vegetation which will create a screen during all seasons of the year.



### Internal Site Illumination

The use of indirect techniques such as up-lighting is encouraged. This will help soften the area at night, and define a unique environment in the Riverfront District. Honoring the older and longer standing architectural traditions of the river front areas when choosing fixtures is encouraged.

Although acceptable, pole lighting is discouraged. All lighting fixtures should be of a type which is in keeping with the historic character of the Character Buildings in the River Core (see above), or with other firmly established historic traditions along the river in the river district.

### Signs

Originality is encouraged. The use of tiles, color, sculpture, paintings, mosaics, etc. is encouraged, as is the use of materials, goods and services

produced in the Riverfront District.

Signs which graphically portray the nature of the business in the building, are encouraged whenever possible. The signs should not be so assertive in color, form, mass, or placement that they overwhelm or detract from the character of the building, or detract from its design nuances. Instead signs should complement the building facade, and add to the overall impression of the building while re-enforcing the building's design. Signs should be placed within architectural features. Sign Materials should be compatible with the facade materials. External illumination is strongly encouraged. Internal illumination, while acceptable, is discouraged.

All freestanding signs other than signs identifying the site as a whole, should conform to the standards on beginning on page fifty six of the Asheville Riverfront Open Space Design Guidelines. Signs which identify the site as a

whole, and which are meant to be read from locations off of the site, should, to the extent possible, honor the spirit of the freestanding sign guidelines found in the Asheville Riverfront Open Space Guidelines. A copy of this section is attached as Attachment D.

### Sidewalks

All sidewalks should be concrete with simple score patterns creating squares. Originality in the use of sculpture, mosaics, tree and planting wells, etc. within the context of the uniform sidewalk standard above, is encouraged, as is the use of materials and goods produced in the Riverfront District.

### Parking

Originality is encouraged. The use of patterned paving, tiles, sculpture, paintings, mosaics, tree wells, etc. in the paving and around the parking area is encouraged, as is the use of materials and goods produced in the Riverfront District. The creative use of landscaping is strongly encouraged.

The number of parking spaces should be minimized. If remote or shared parking is available, it should be utilized to the maximum extent possible. The parking areas should be located such that they have a minimal interaction with foot traffic in the area.

All parking areas should be pleasant and feel inviting. Plantings should be used between the street and the parking area such that any view of automobiles is filtered. It is preferable if nothing below the bottom of the windshields is visible. These same standards should be applied to parking areas and their relation to interior

spaces. Shade trees appropriate for the Riverfront District, should be planted so that the lots remain cool and pleasant. Up-lighting the trees and surrounding buildings to create a unique environment and help definition and safety at night is strongly recommended.

### Utilities

All utilities should be placed underground.

### Street lighting

All street lighting should be of a uniform character complementing the character of long standing Architectural traditions in the immediate area.

### Street Furniture

The use and placement of public furniture such as benches, trash receptacles, tables, planters, drinking fountains etc. is encouraged.

Originality is encouraged. The use of color, tiles, sculpture, paintings, mosaics, etc. is encouraged, as is the use of materials and goods produced in the Riverfront District. The creative use of landscaping is also encouraged.

Unless the particular piece of furniture is a unique original piece of art, it should respect and compliment the ambiance established by the Character Buildings in the area. If the furniture is an original and unique piece of art, it should respect the character of the area.

### Sculptural Art

The use of sculpture, fountains, etc. is strongly encouraged. Originality in materials, placement and execution is also highly encouraged. The use of materials and goods produced in the river front area is strongly recommended.



### Other

The placement of Kiosks is highly encouraged. Treatment of the kiosks as public sculpture enhancing the character of the area is also encouraged.

Larger roofed but non-walled spaces designed to be entered should conform to historical patterns in the area that are compatible with the character of the area. Liberal use of Landscaping to define these areas is encouraged.

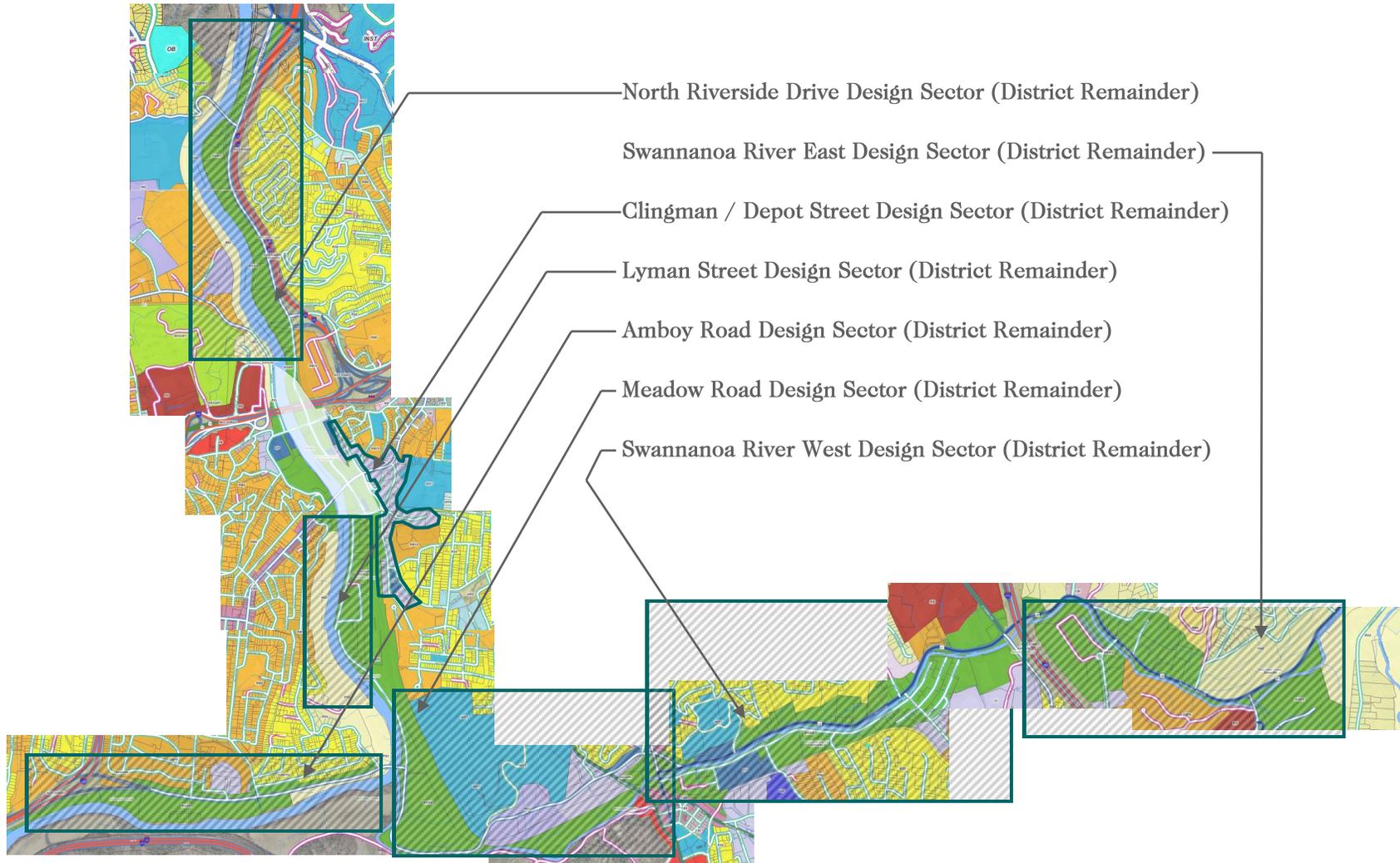
Structures built in or built to appear as though they are in the French Broad or Swannanoa Rivers should conform to all the above guidelines for all parts of the project which may be on land except as noted below. Docks not originating in the planned open space areas of the Riverfront District should honor the history of the area in materials, form, size, and color, as should any structure associated with the docks, either directly over water, or in close proximity to the docks. If the use of the project is to be primarily entertainment or water oriented uses, then it is possible that a more appropriate character building would be those structures used in the early part of the 20th century in the recreational

Photographs of these uses are available, and they are the basis of many of the structures detailed in the Asheville Open Space Design Guidelines.

Any structure connected with the land bordering the French Broad River which is also in the planned open space areas of the River District should conform to the Asheville Riverfront Open Space Design Guidelines.

Structures in the River proper should conform to the above guidelines unless constructed to appear as a boat. If the structure resembles a boat, then it should have a direct visual relationship in size, color, and shape to historical boat styles as have been used on the French Broad River. If an actual water craft is used for commercial, residential or decorative purposes and is in the French Broad River or used in extremely close proximity to the River such that it appears to be in the French Broad River, it should have a direct visual relationship in size, color, and shape to water craft actually used on the French Broad River in times past. A festive and inviting atmosphere celebrating the river and its past is encouraged.

# Remainder District Design Sectors



# North Riverside Drive

This Remainder Design District encompasses an area along the French Broad River, bisected by the Norfolk Southern Railway in most areas and extending along Riverside Drive from the Norfolk Southern Trestle over the French Broad at the South end to the intersection of Broadway and Riverside Drive at the North end.

The area is characterized by a mixture of small commercial enterprises, river sports, warehouses, industrial and light industrial.





Images which could provide a visual cue as thematic inspiration for potential design concepts utilized in future redevelopment of this area.



## Clingman / Depot Street

This Remainder Design District encompasses an area extending along a portion of Roberts Street at the North end continuing southward to include a portion of Clingman Avenue and Extension and continuing on Depot Street to the South.

The area is characterized by a mixture of uses including industrial, light industrial, commercial, artist studios, retail, and a residential component.

This area has seen the most dynamic redevelopment and serves as a good model for a successful look and feel.





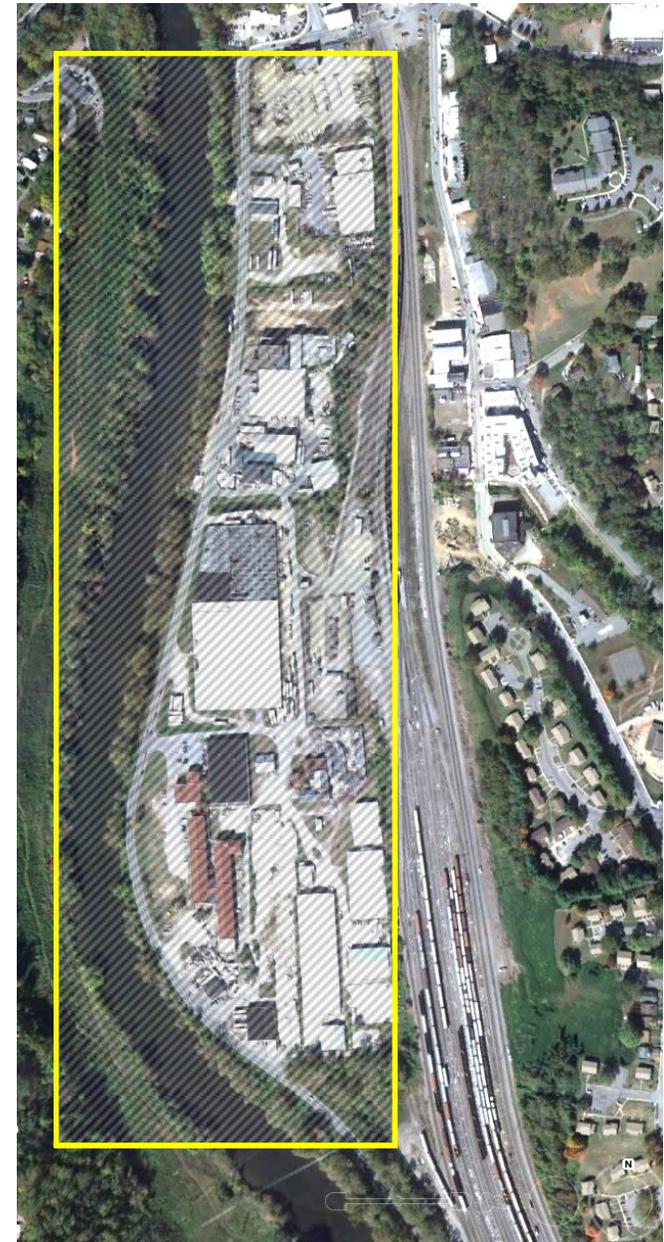
Images which could provide a visual cue as thematic inspiration for potential design concepts utilized in future redevelopment of this area.



# Lyman Street

This Remainder Design District encompasses an area along the French Broad River, extending along Lyman Street from the intersection with Riverside Drive to the North to the Amboy bridge over the French Broad River at the South end.

The area is characterized by a mixture of operating heavy and light industrial uses along with warehouses, artist studios, and commercial.





Images which could provide a visual cue as thematic inspiration for potential design concepts utilized in future redevelopment of this area.

# Amboy Road

This Remainder Design District encompasses an area along the French Broad River extending along Amboy Road from the Amboy bridge at the East end, to intersection of Amboy Road and Interstate 240.

The area is characterized mostly by the great expanse of greenway and outdoor recreational uses. There are a few existing operating commercial and industrial uses.

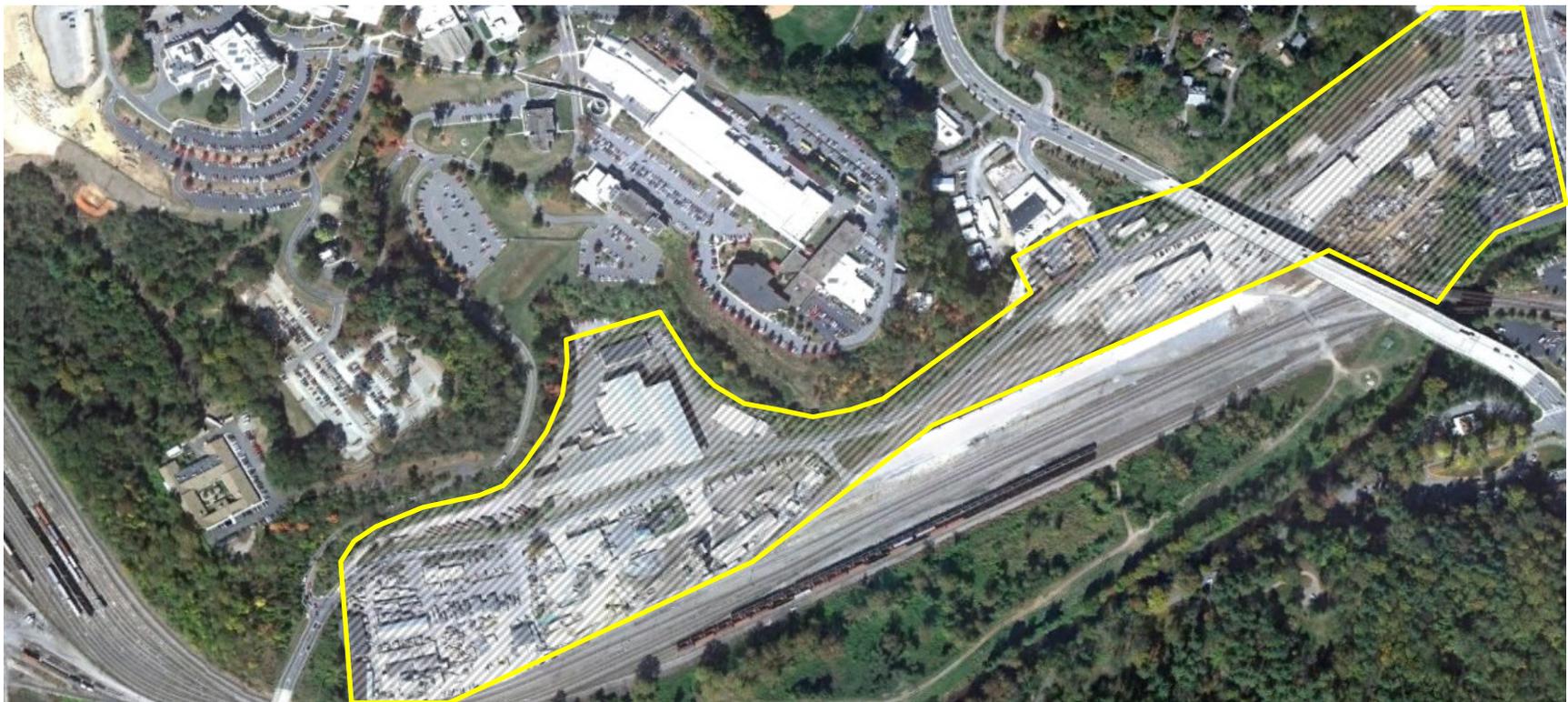




Images which could provide a visual cue as thematic inspiration for potential design concepts utilized in future redevelopment of this area.

## Meadow Road

This Remainder Design District encompasses an area along the Swannanoa River, separated by the Norfolk Southern Railway extending along Meadow Road from the overpass near Victoria Road at the West, to Biltmore Avenue to the East. The area is characterized by a mixture of operating industrial uses along with a nonprofit retail business (Habitat for Humanity) and a metals recycling operation.





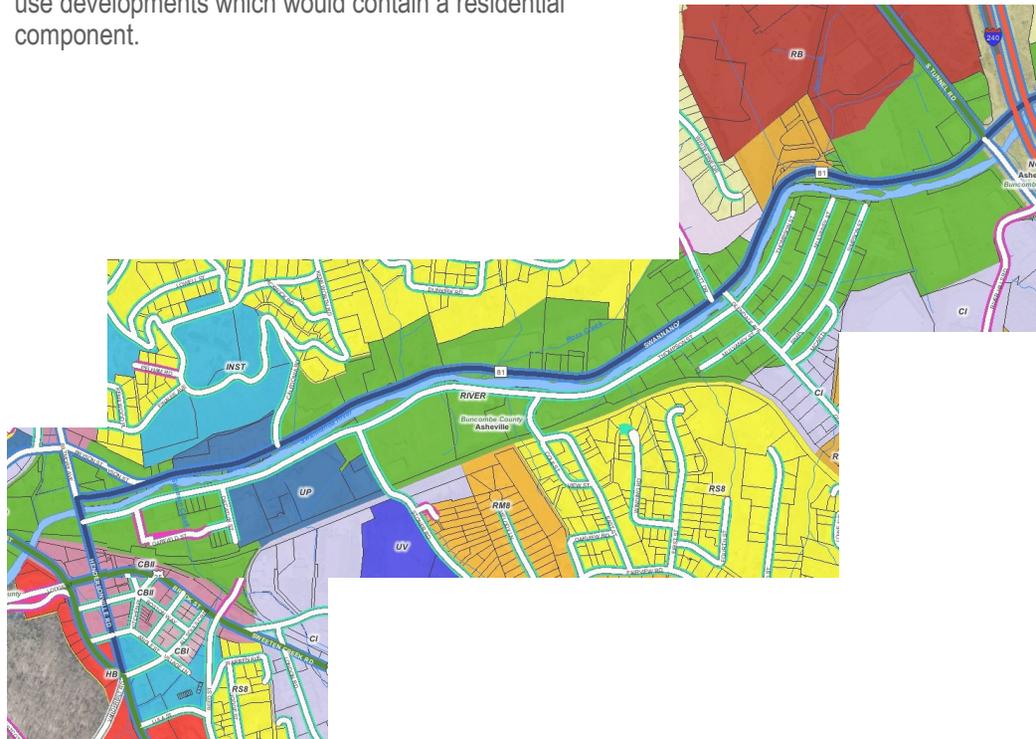
Images which could provide a visual cue as thematic inspiration for potential design concepts utilized in future redevelopment of this area.



## Swannanoa River West

This Remainder Design District encompasses a large area fronting both banks of the Swannanoa River, and extending along Swannanoa River Road and Thompson Street, from Biltmore Avenue on the West to the I-240 overpass at South Tunnel Road at the East end.

The area is characterized by a mixture of existing operating light industrial uses, retail, and warehousing. Even though there is not currently any residential uses within this area, there has been some interest in mixed use developments which would contain a residential component.





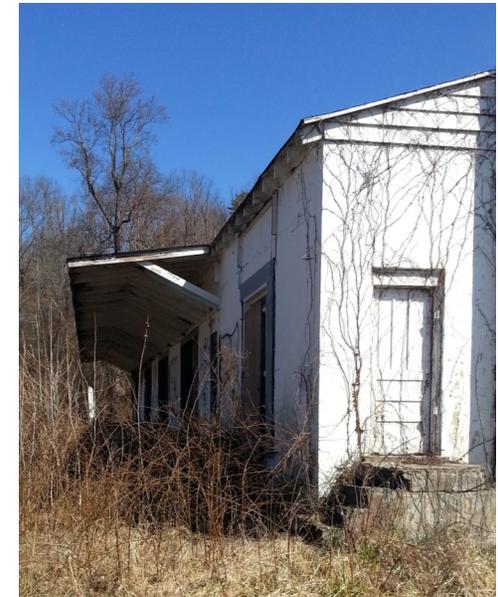
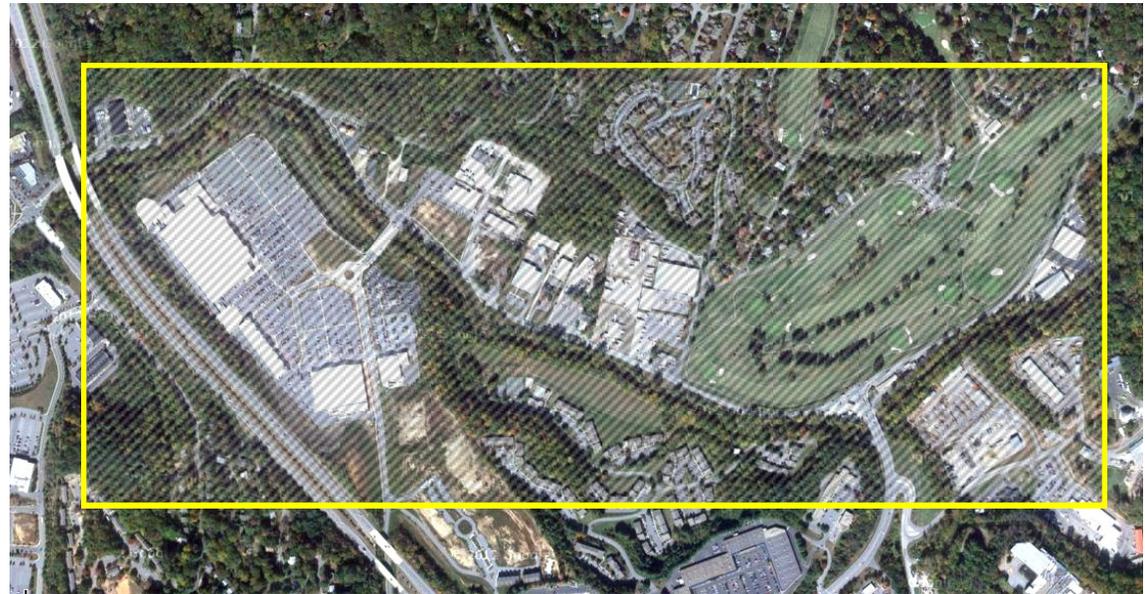
Images which could provide a visual cue as thematic inspiration for potential design concepts utilized in future redevelopment of this area.



# Swannanoa River East

This Remainder Design District encompasses a large area fronting both banks of the Swannanoa River, and extending along Swannanoa River Road beginning at the I-240 overpass at South Tunnel Road at the West end, to the Nature Center of Asheville

The area is characterized by a mixture of existing operating light industrial uses, retail, commercial, and warehousing.





Images which could provide a visual cue as thematic inspiration for potential design concepts utilized in future redevelopment of this area.





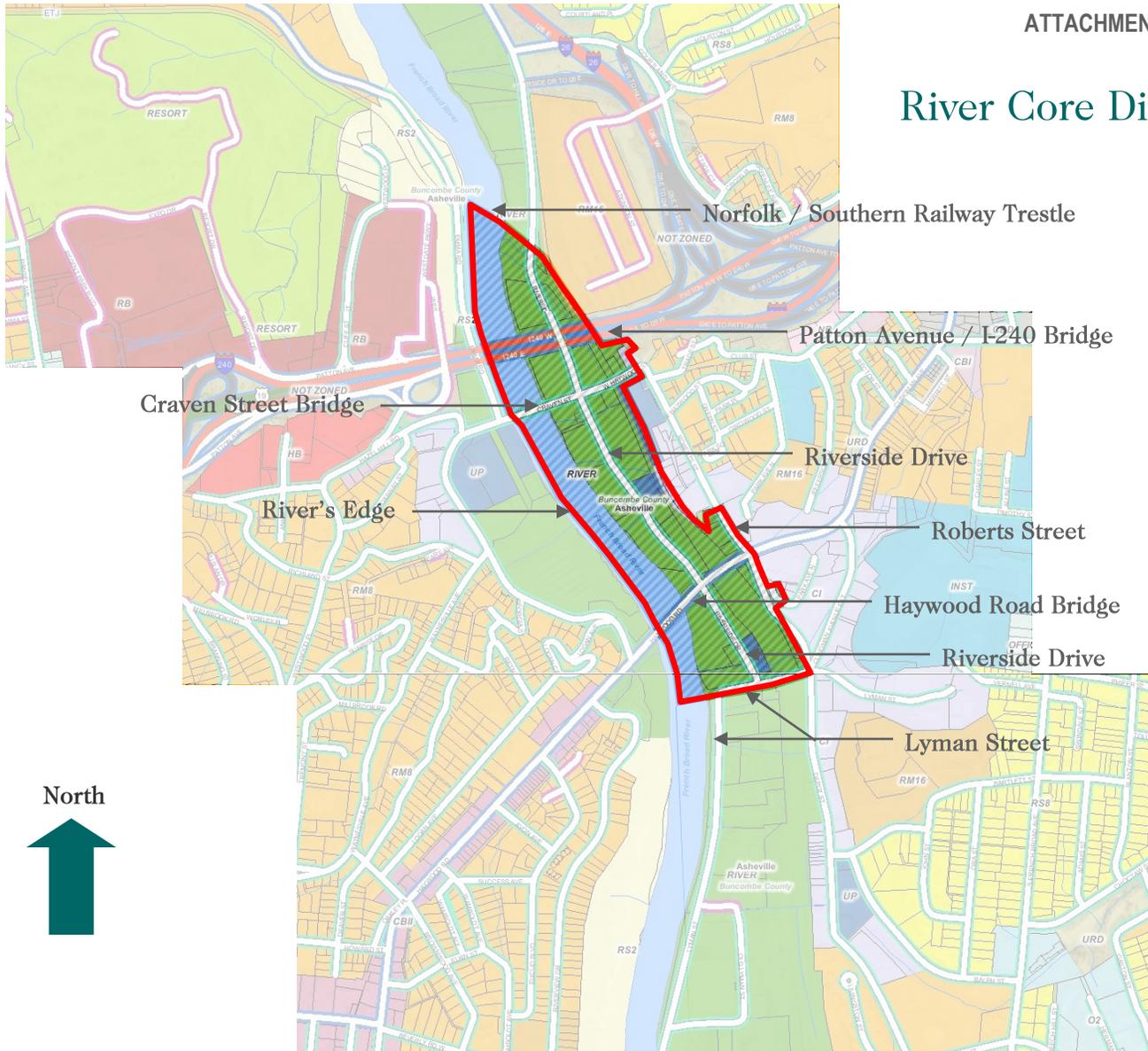
## Appendices

**A River Core District Map**

**B Character Buildings**

Reference is hereby made to the Unifide Development Ordinance for the City of Asheville relative to other requirements affecting your project proposal - such as, but not limited to topics concerning, zoning classifications, density, open space requirements, parking, lighting, signage, flood hazard areas development, etc.

# River Core District Map



ATTACHMENT “B”

**Character Buildings**  
for the River Core Design District



Warehouse Studios Building  
Intersection of Lyman Street and  
Riverside Drive

Built 1925



Curve Studios  
6, 9, 12 Riverside Drive

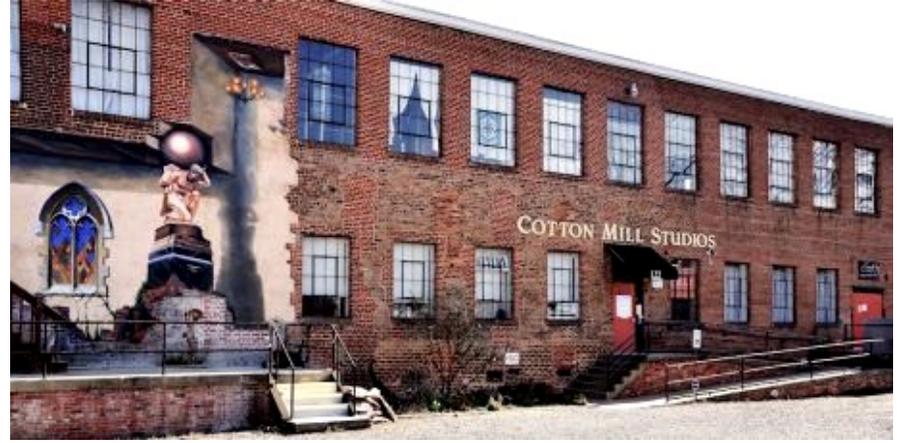




PSNC Building

233 Riverside Drive





**Cotton Mill Studios**  
**122 Riverside Drive**





**Remnants of Old Cotton Mill**

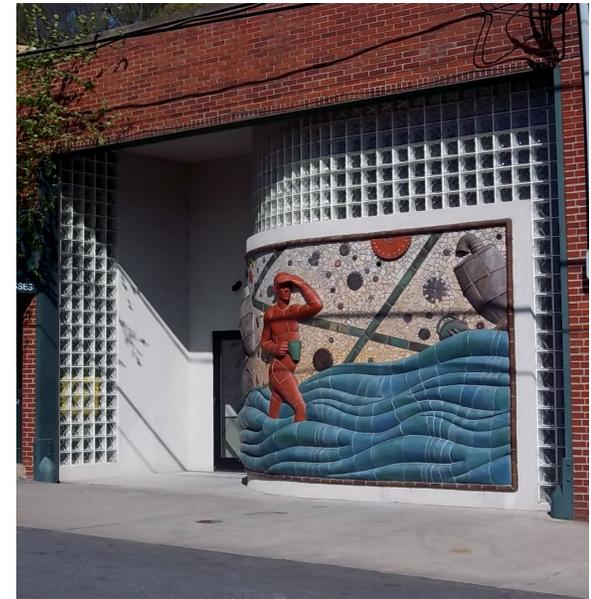
**1205 Riverside Drive**



**Riverside Studios**

**174 Craven Street**

**East of the Railroad Tracks and South of Haywood Street**



Odyssey Center for the Ceramic Arts  
236 - 244 Clingman Ave. Extension



Wedge Studios Building  
123 - 133 Roberts Street



**East of the Railroad Tracks and North of Haywood Street**



Adjacent to Phil Mechanic Studios





**Phil Mechanic Studios**

**109 Haywood Street**